

TOWNSHIP OF CHATHAM, MORRIS COUNTY, NEW JERSEY

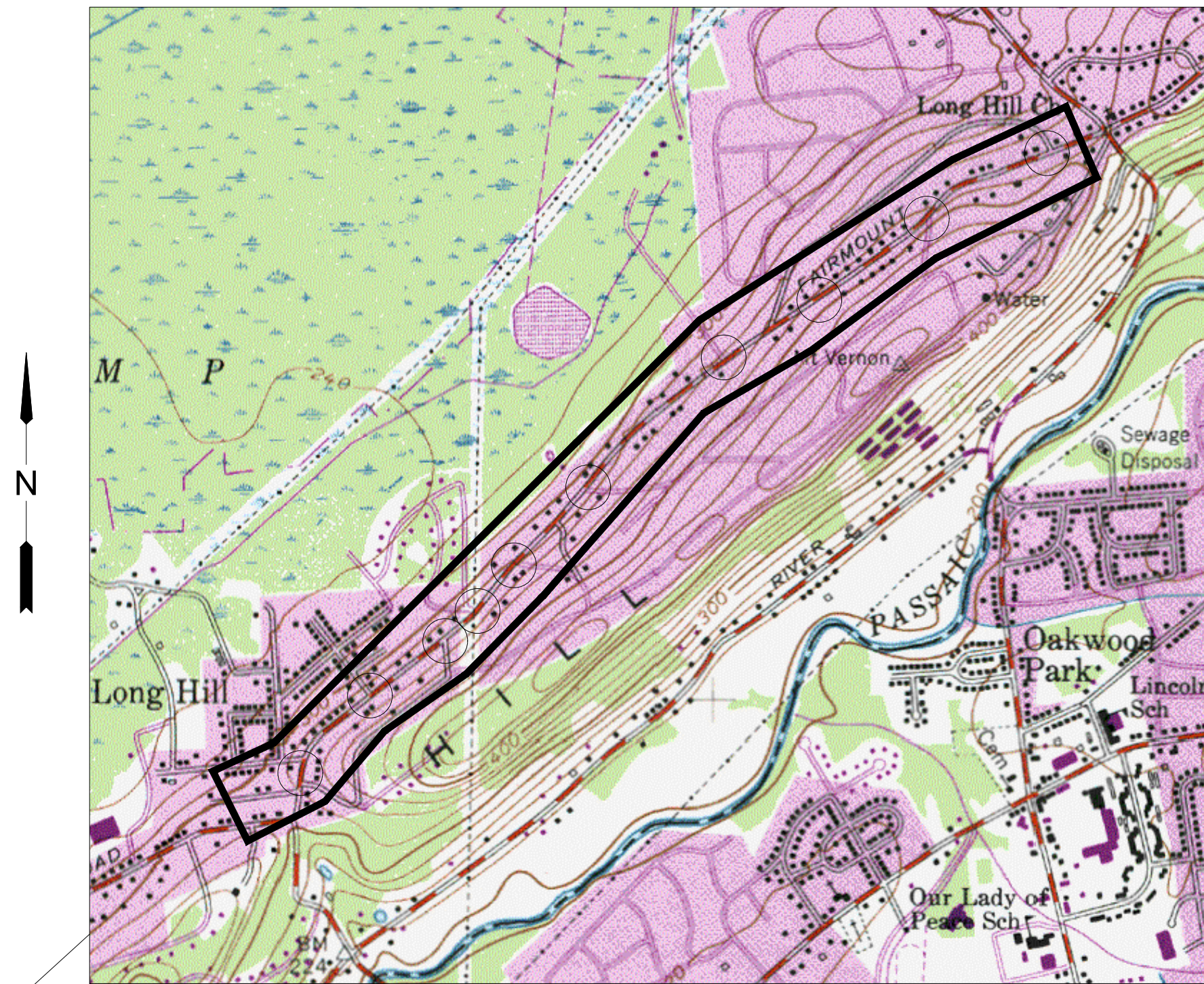
PEDESTRIAN SAFETY AND ADA IMPROVEMENTS

MAYOR
Mayor Ashley Felice

TOWNSHIP COMMITTEE
Mark Hamilton
Stacy Ewald
Mike Choi
Mark Lois

TOWNSHIP ADMINISTRATOR
Ziad Andrew Shehady

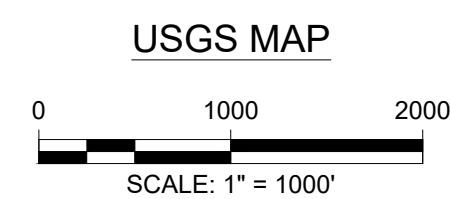
TOWNSHIP ENGINEER
John K. Ruschke, P.E.



SOURCE: USGS QUADRANGLE: CHATHAM, MORRISTOWN

PROJECT LOCATION

TOTAL LENGTH OF THE PROJECT AREA = 9000 LF OR 1.7 MILES



AUGUST 2023

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MOTT
MACDONALD

JOHN K. RUSCHKE
Professional Engineer - N.J. Lic. No. 24GE0037148

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GENERAL NOTES:

1. ALL WORK SHALL BE PERFORMED IN ACCORDANCE WITH THESE PLANS AND SPECIFICATIONS AND ALL APPLICABLE REQUIREMENTS AND STANDARDS OF THE COUNTY OF MORRIS.
2. HOURS OF OPERATION FOR THIS PROJECT SHALL BE WITHIN THE HOURS OF 9:00 AM TO 4:00 PM UNLESS PRIOR APPROVAL IS GRANTED DURING THE PRECONSTRUCTION MEETING.
3. THE ENGINEER NOR HIS REPRESENTATIVES ARE RESPONSIBLE FOR JOB SAFETY. JOB SAFETY AND THE SAFETY OF THE PUBLIC IS THE SOLE RESPONSIBILITY OF THE CONTRACTOR.
4. THE CONTRACTOR SHALL CONTACT NJ ONE CALL (1-800-272-1000) PRIOR TO THE COMMENCEMENT OF ANY WORK.
5. MINIMUM, ONE WEEK PRIOR TO THE START OF THE PROJECT, CONTRACTOR SHALL HAVE PROPER ROAD CONSTRUCTION SIGNS IN PLACE, AS DESIGNATED AND APPROVED BY THE ENGINEER.
6. INLETS AND MANHOLES TO BE RESET SHALL INCLUDE, IF NECESSARY, REPLACING OF ANY DETERIORATING BLOCK OR BRICK FOUND IN THE UPPER THREE COURSES OF THE STRUCTURE.
7. PRICE BID FOR RECONSTRUCT AND RESET INLET OR MANHOLE TO INCLUDE RESETTING OF CASTING TO APPROPRIATE ELEVATION IF REQUIRED.
8. RESPECTIVE UTILITY COMPANIES SHALL BE RESPONSIBLE FOR THE RELOCATION OF THEIR STRUCTURES AND THE RAISING OF THEIR VALVES, BOXES, AND MANHOLES. CONTRACTOR TO COORDINATE WITH UTILITY COMPANIES TO IDENTIFY ANY BURIED CASTINGS OR VALVES. REGARDLESS OF RESPONSIBILITY THE CONTRACTOR SHALL ENSURE THAT ALL VALVES, BOXES, AND MANHOLES HAVE BEEN IDENTIFIED AND RAISED.
9. ALL PROJECTS WILL BE SUBJECT TO THE HOLIDAY SCHEDULE AS RECOGNIZED BY THE COUNTY OF MORRIS.
10. ALL NEW CURB PIECES TO CONFORM TO CAMPBELL TYPE 'N-Eco' OR EQUIVALENT, WITH "DUMP NO WASTE - DRAINS TO WATERWAYS" CAST INTO ALL CURB PIECES AND BICYCLE SAFE GRATES.
11. ANY INLET CONTAINING AN INLET RISER AND REQUIRING SPACERS FOR CURB PIECES, COST OF SPACERS AND INSTALLATION SHALL BE INCLUDED IN PRICE BID FOR CURB PIECE, CONTRACTOR TO DETERMINE ALL LOCATIONS. EXISTING RISERS AND SPACERS IN INLETS TO BE RECONSTRUCTED OR RESET SHALL BE REMOVED AND CASTINGS SET TO PROPER ELEVATION.
12. WHERE REPLACING SIGN POST, PROTECT/REUSE EXISTING SIGN. SIGNS DAMAGED BY CONTRACTOR ARE TO BE REPLACED AT NO ADDITIONAL COST.
13. CONTRACTOR IS RESPONSIBLE FOR CONSTRUCTION LAYOUT AS NECESSARY.
14. ALL SIGNAGE AND LANE STRIPING IS TO BE INSTALLED IN ACCORDANCE WITH THE MOST CURRENT VERSION OF THE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES (MUTCD).
15. AT NO ADDITIONAL COST, THE CONTRACTOR IS RESPONSIBLE TO APPLY FOR AND OBTAIN A ROAD OPENING PERMIT FROM THE COUNTY OF MORRIS. INSURANCE CERTIFICATE AND A PERFORMANCE BOND MEETING THE COUNTY REQUIREMENTS SHALL BE SUBMITTED.

CURB NOTES:



1. CONSTRUCTION OF ALL CURBS SHALL CONFORM TO NJDOT STANDARDS AND SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION, 2007, SECTION 607, AND SUPPLEMENTS THERETO, AND SHALL MEET THE TOWNSHIP ENGINEER'S APPROVAL. ALL CONCRETE TO BE USED SHALL BE CLASS B AS SPECIFIED IN NJDOT STANDARDS AND SPECIFICATIONS THERETO.
2. GRANITE BLOCKS SHALL HAVE A 10 INCH TO 12 INCH VERTICAL LENGTH.
3. CURBS SHALL BE SET IN A CLASS B CONCRETE BASE, AT LEAST FOUR INCHES THICK IN FRONT AND REAR OF GRANITE BLOCK AND AT LEAST 6 INCHES THICK BELOW THE BLOCK. THE COMBINED BLOCK AND CONCRETE BASE SHALL HAVE A MINIMUM VERTICAL DIMENSION OF 18 INCHES.
4. CURBS SHALL HAVE A SIX INCH EXPOSED VERTICAL FACE ABOVE THE FINISHED PAVEMENT SURFACE.
5. JOINTS SHALL MEASURE 3/8" INCH TO 1/2" IN WIDTH.
6. THE CURB HEIGHT AT ALL DRIVEWAY OPENINGS SHALL BE LOWERED TO A POINT TWO INCHES ABOVE THE FINISHED SURFACE.
7. THE CURBING SHALL BE DESIGNED TO PROVIDE BARRIER FREE CURB RAMPS CONSTRUCTED IN ACCORDANCE WITH DESIGN STANDARDS FOR CURB RAMPS FOR THE PHYSICALLY HANDICAPPED OF THE NEW JERSEY DEPARTMENT OF TRANSPORTATION.

SIDEWALK NOTES:

1. A MINIMUM WALKWAY WIDTH OF FOUR FEET SHALL BE CONSTRUCTED IN ACCORDANCE WITH THE REQUIREMENTS OF THE TOWNSHIP ENGINEER IN THE CONFORMANCE WITH CHAPTER 19 OF THE CODE OF THE TOWNSHIP OF CHATHAM.
2. WHERE EXTRA WIDTH HAS BEEN DEDICATED FOR WIDENING OF EXISTING STREETS, LOTS SHALL BEGIN WITH SUCH NEW LINE AND ALL SETBACKS SHALL BE MEASURED FROM SUCH NEW LINE.
3. CURBING SHALL BE REPAIRED WHEREVER DAMAGED DURING CONSTRUCTION AT NO ADDITIONAL COST. BELGIAN (GRANITE) BLOCK CURBS SHALL BE USED ON ALL TOWNSHIP STREETS ACCORDING TO THE FOLLOWING STANDARDS:
 - 3.1. CONSTRUCTION OF ALL CURBS SHALL CONFORM TO NJDOT STANDARDS AND SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION, 2007, SECTION 607, AND SUPPLEMENTS THERETO, AND SHALL MEET THE TOWNSHIP ENGINEER'S APPROVAL. ALL CONCRETE TO BE USED SHALL BE CLASS B AS SPECIFIED IN NJDOT STANDARDS AND SPECIFICATIONS THERETO.
 - 3.2. GRANITE CURBS SHALL HAVE A 10 INCH TO 12 INCH VERTICAL LENGTH.
 - 3.3. CURBS SHALL BE SET IN A CLASS B CONCRETE BASE AT LEAST FOUR INCHES THICK IN FRONT AND REAR OF THE GRANITE BLOCK AND AT LEAST 6 INCHES THICK BELOW THE BLOCK. THE COMBINED BLOCK AND CONCRETE BASE SHALL HAVE A MINIMUM VERTICAL DIMENSION OF 18 INCHES.
 - 3.4. CURBS SHALL HAVE A SIX INCH EXPOSED VERTICAL FACE ABOVE THE FINISHED PAVEMENT SURFACE.
 - 3.5. JOINTS SHALL MEASURE 3/8" TO 1/2" IN WIDTH.
 - 3.6. THE CURB HEIGHT AT ALL DRIVEWAY OPENINGS SHALL BE LOWERED TO A POINT TWO INCHES ABOVE THE FINISHED PAVEMENT.
 - 3.7. THE CURBING SHALL BE DESIGNED TO PROVIDE BARRIER FREE CURB RAMPS CONSTRUCTED IN ACCORDANCE WITH THE DESIGN STANDARDS FOR CURB RAMPS FOR THE PHYSICALLY HANDICAPPED OF THE NEW JERSEY DEPARTMENT OF TRANSPORTATION.

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	412 Mount Kemble Avenue Suite G22 Morristown NJ 07960 United States of America Certificate No. 24GA28016600	Client TOWNSHIP OF CHATHAM MORRIS COUNTY, NEW JERSEY	JOHN K. RUSCHKE PROFESSIONAL ENGINEER LIC. NO. 24GE0037148  Date 08-03-2023	Designed RS 05/23 Drawn RS 05/23 Dwg check SJA 05/23 Scale at ARCH D N.T.S. Status Draft Rev Rev0 Security STD	Eng check SJA 05/23 Coordination Approved JKR 05/23 Rev Rev0 Security STD	Title CHATHAM TOWNSHIP PEDESTRIAN SAFETY AND ADA IMPROVEMENTS GENERAL NOTES & DETAILS
	Project Number 507393101-001		B/O 2	Total 19	Drawing Number G-002	
Rev	Date	Drawn	Description	Ch'k'd	App'd	



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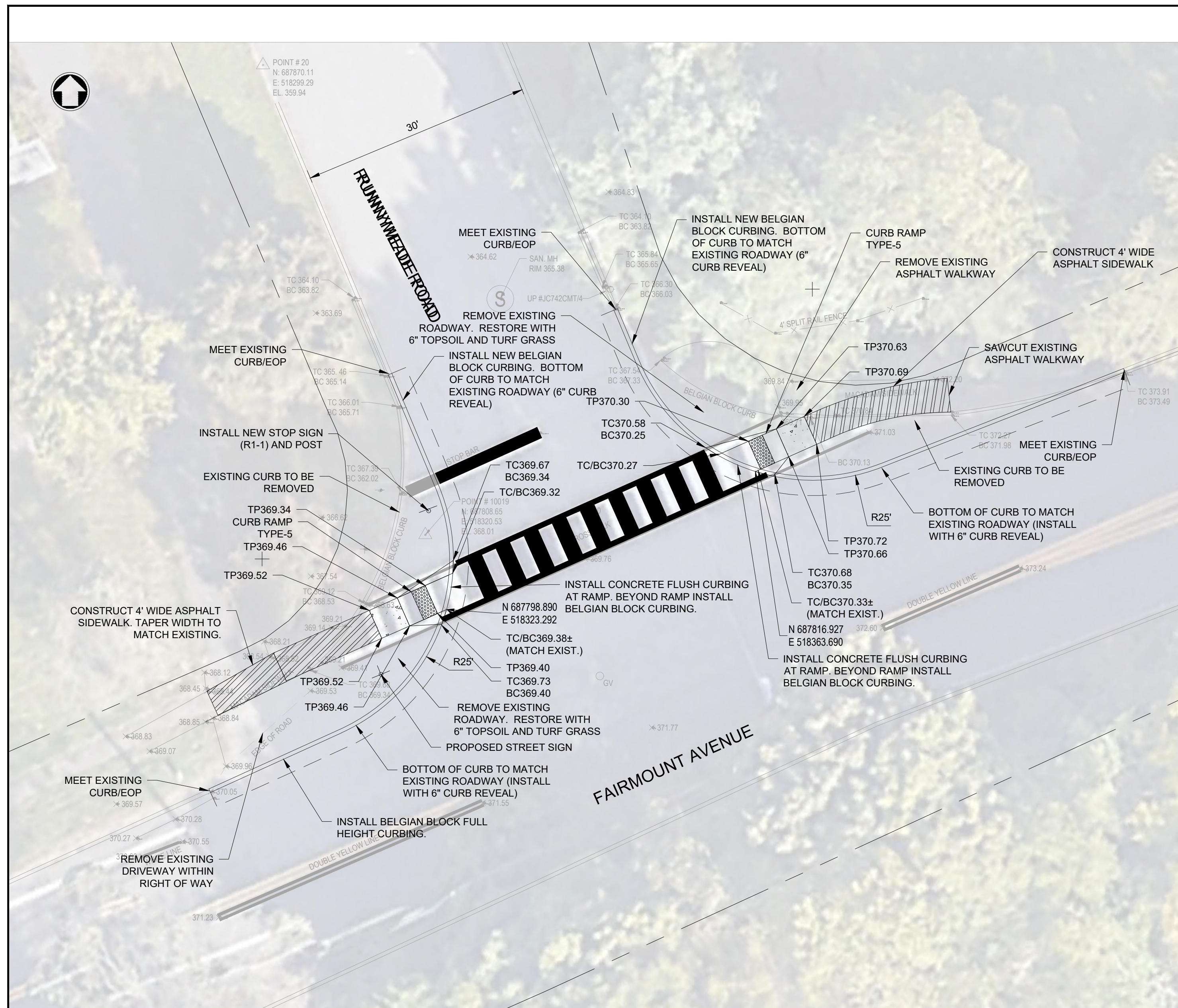
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 Date 08-03-2023

Project Number **507393101-001** B/O Total **19**

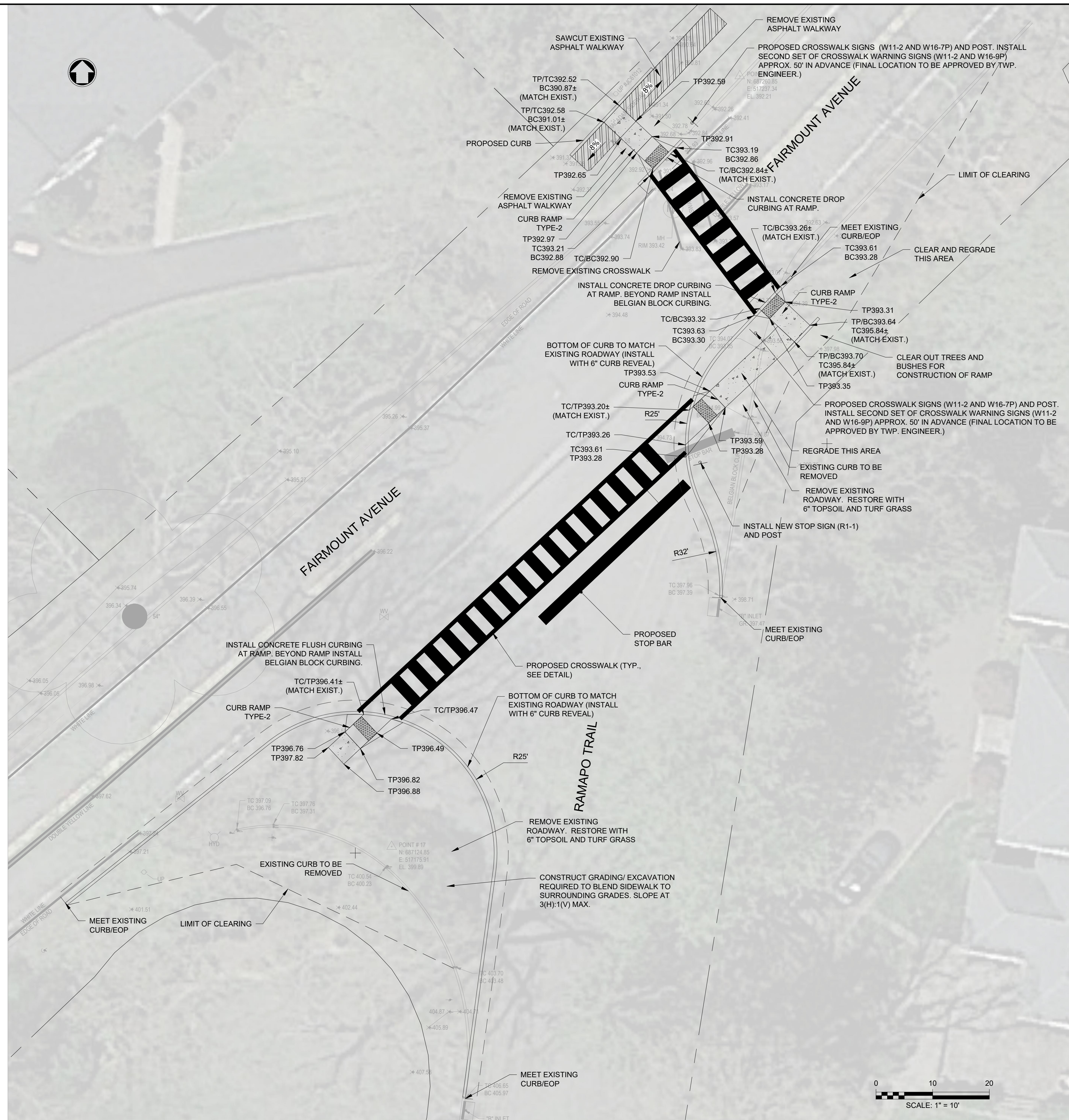
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Drawing Number		C-100			

Title
**CHATHAM TOWNSHIP
 PEDESTRIAN SAFETY AND ADA
 IMPROVEMENTS**

OVERALL SITE PLAN



**CURB RAMP DETAIL A
RUNNYMEDE ROAD & FAIRMOUNT AVENUE**



FAIRMOUNT AVENUE & RAMAPO TRAIL

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John Ruschke
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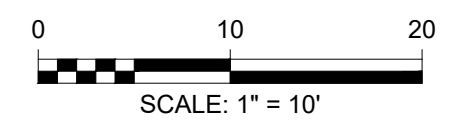
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Drawing Number		C-101			

Title
**CHATHAM TOWNSHIP
PEDESTRIAN SAFETY AND ADA
IMPROVEMENTS**

SITE PLAN



FAIRMOUNT AVENUE & ROCKLEDGE TRAIL



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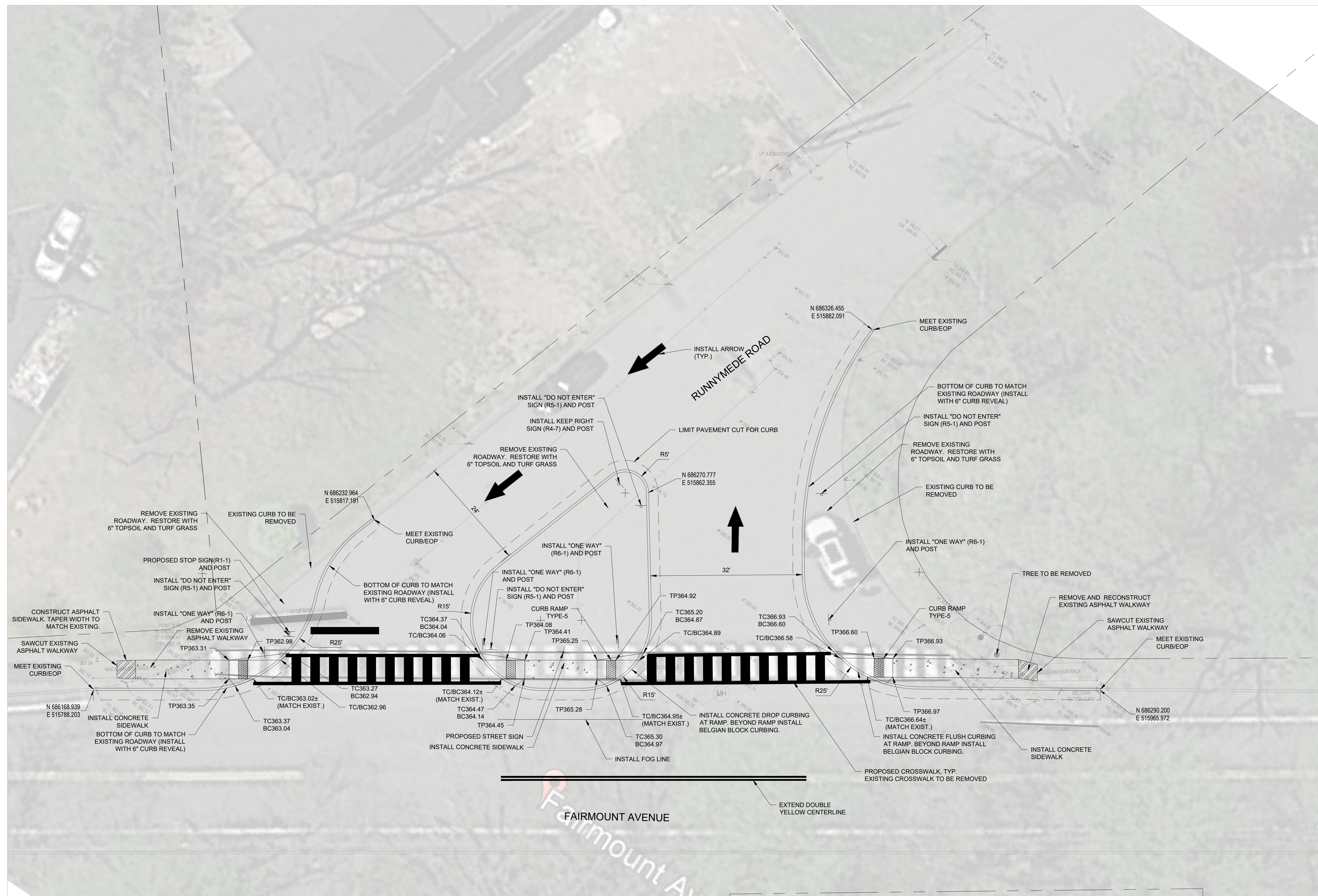
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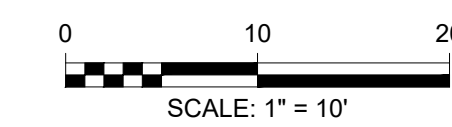
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1"=10'	Draft	Rev0	STD		
Drawing Number	C-102				

Title
CHATHAM TOWNSHIP
PEDESTRIAN SAFETY AND ADA
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SITE PLAN



FAIRMOUNT AVENUE & RUNNYMEDE ROAD



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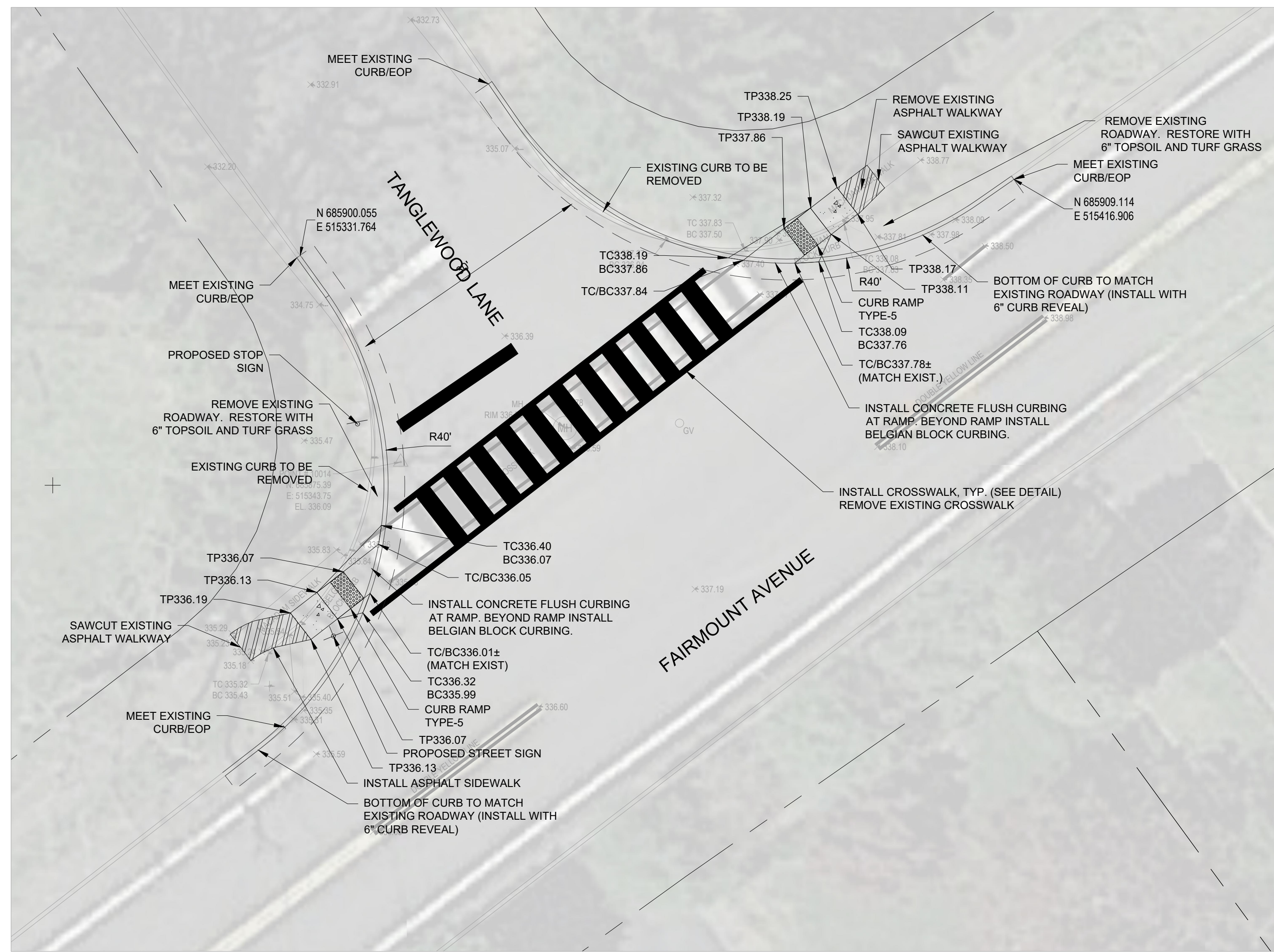
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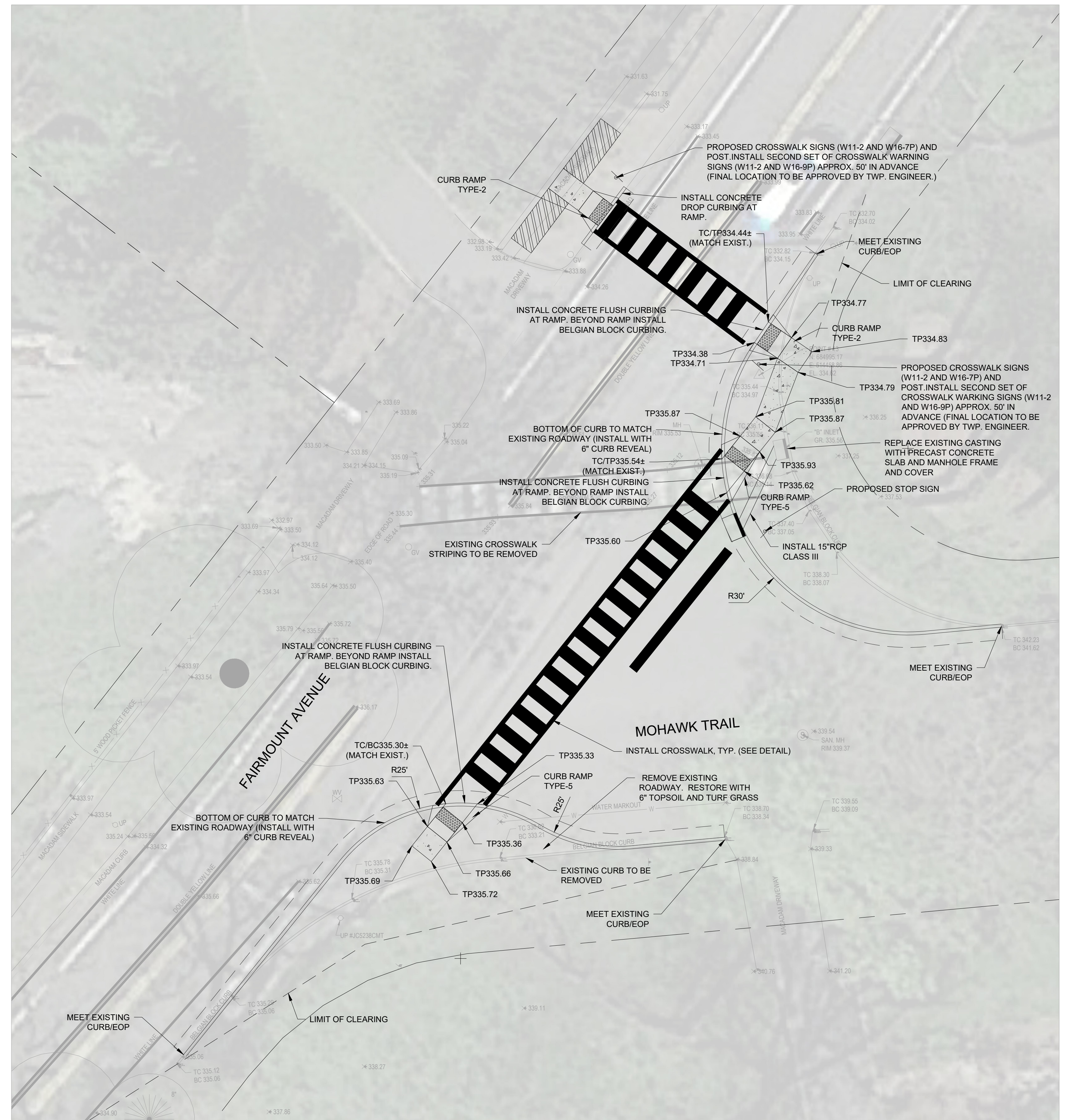
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Title **CHATHAM TOWNSHIP
 PEDESTRIAN SAFETY AND ADA
 IMPROVEMENTS**

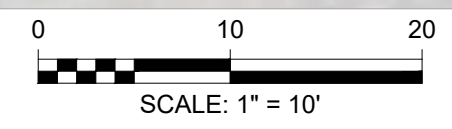
SITE PLAN



FAIRMOUNT AVENUE & TANGLEWOOD LANE



FAIRMOUNT AVENUE & MOHAWK TRAIL



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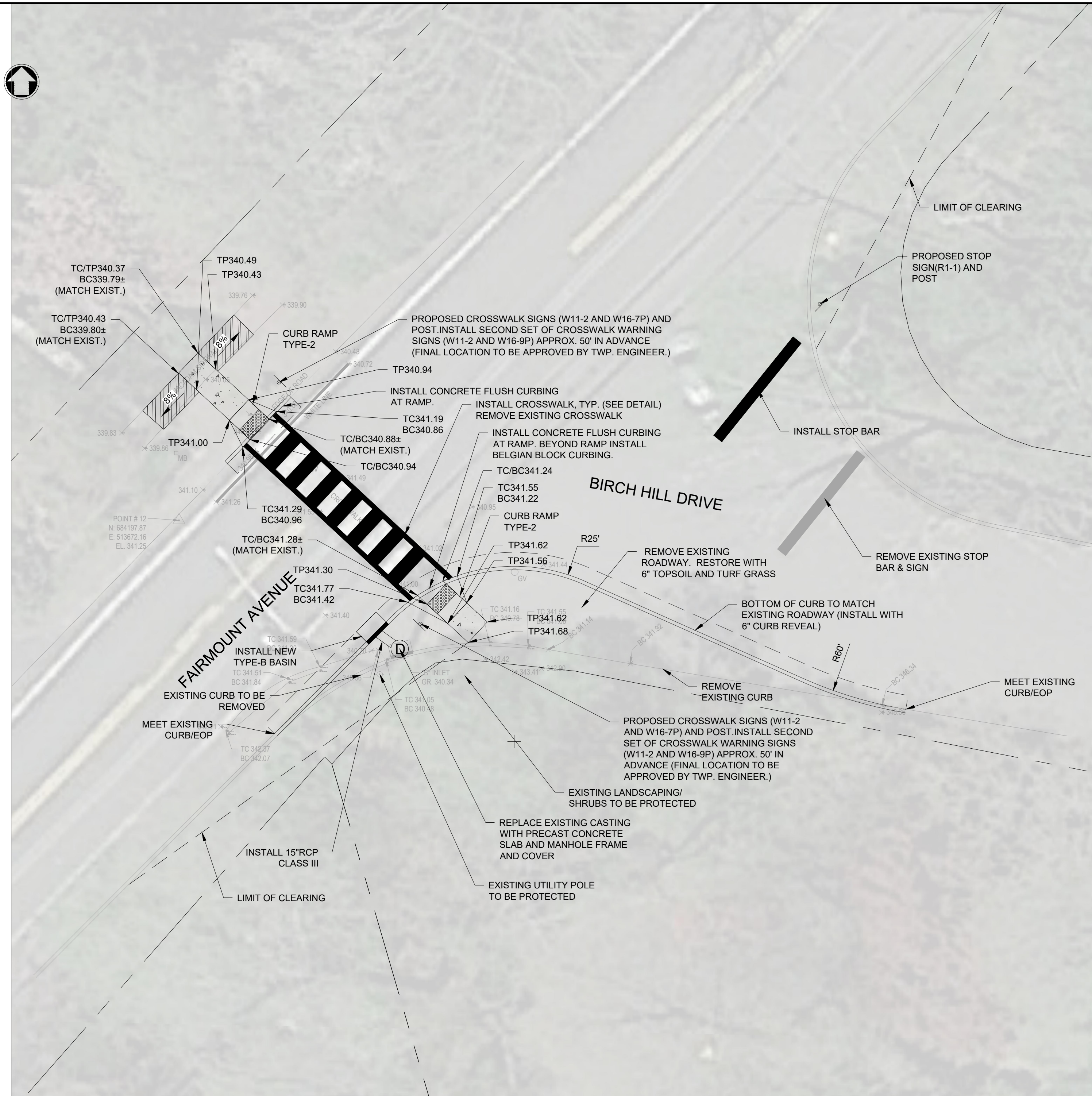
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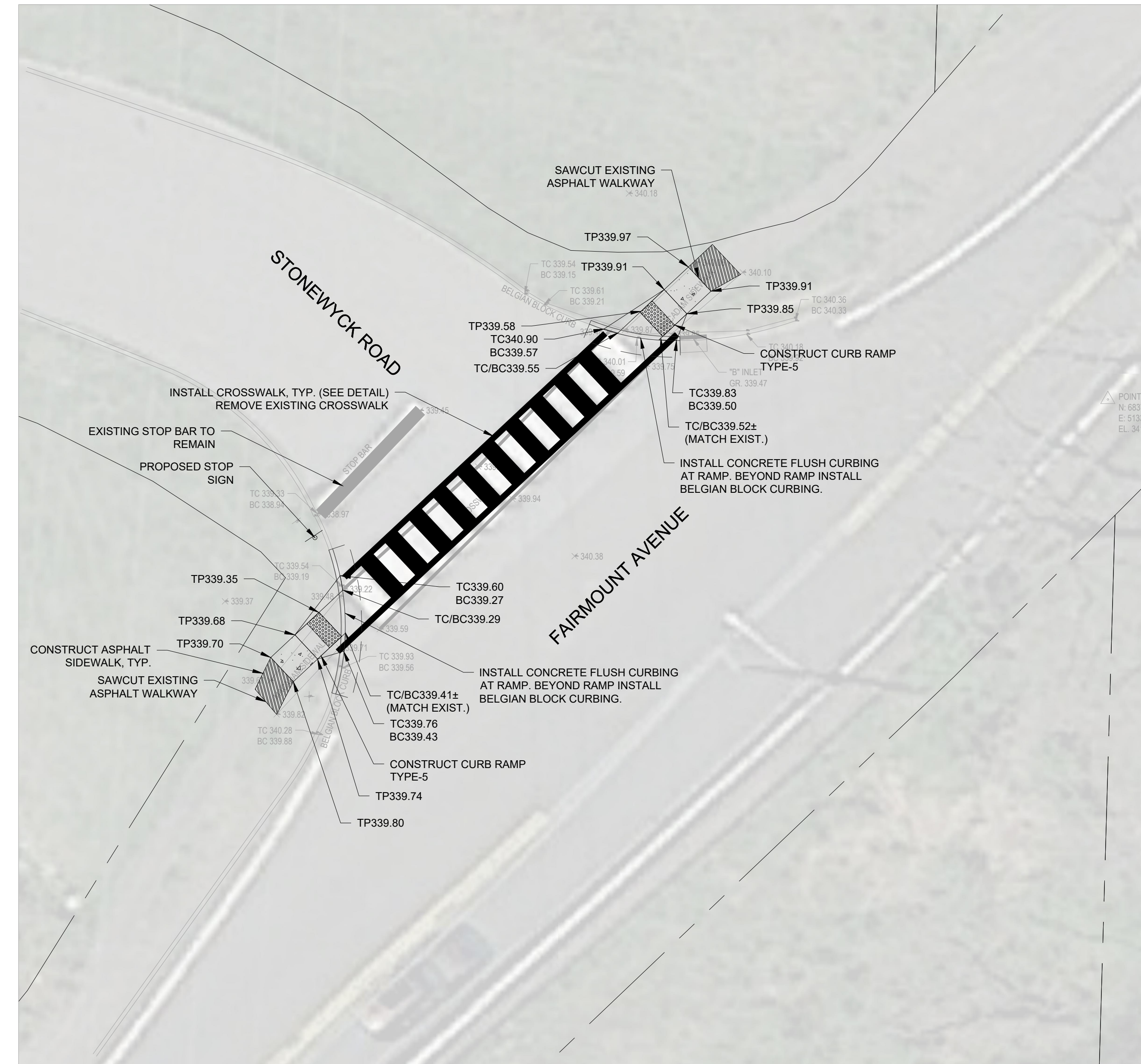
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Drawing Number	C-104				

Title: **CHATHAM TOWNSHIP
 PEDESTRIAN SAFETY AND ADA
 IMPROVEMENTS**

SITE PLAN



FAIRMOUNT AVENUE & BIRCH HILL DRIVE



FAIRMOUNT AVENUE & STONEYWYCK ROAD



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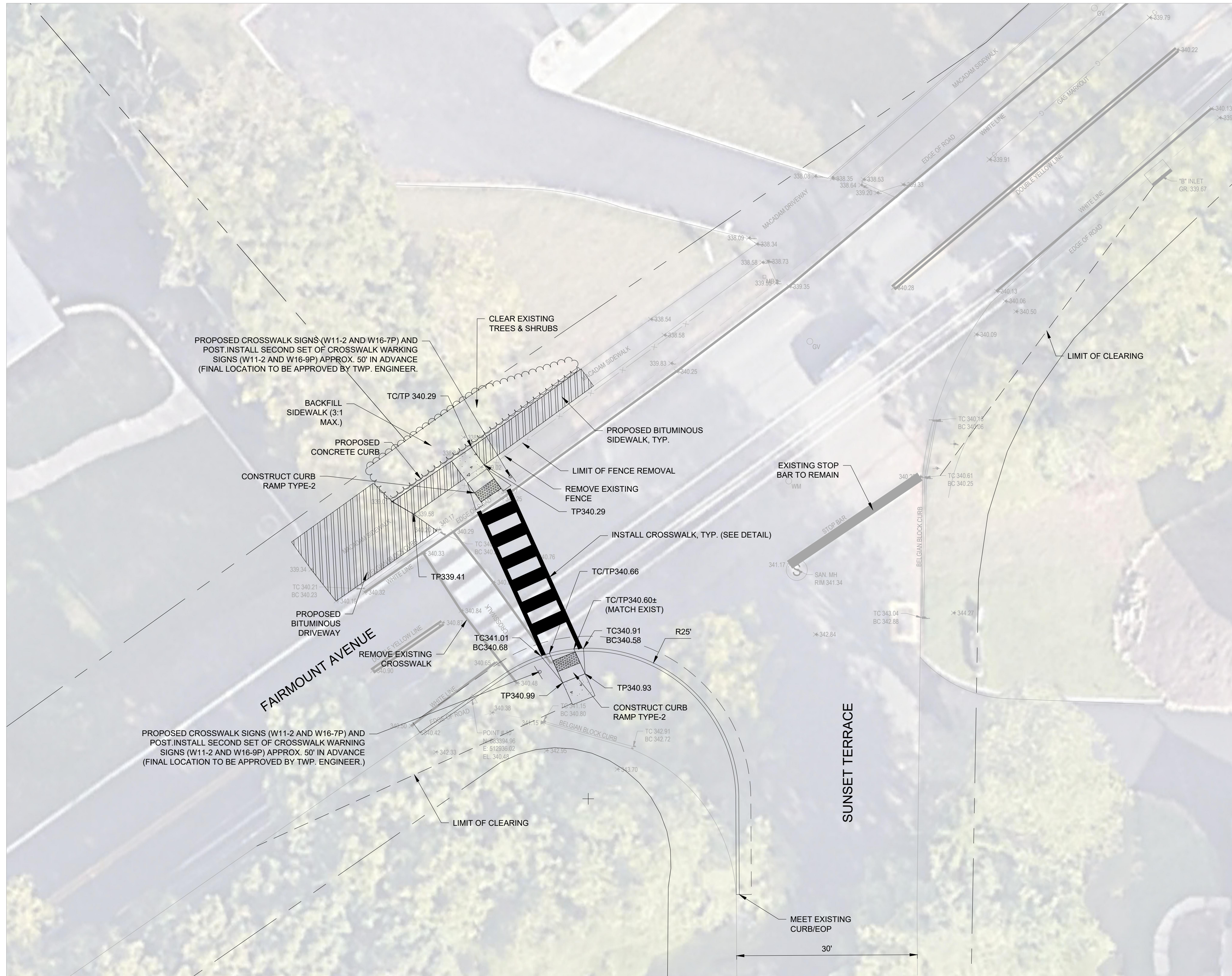
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B/O **8** Total **19**

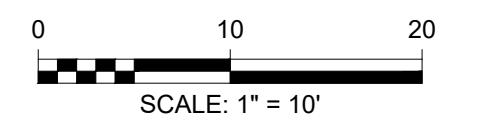
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Title
CHATHAM TOWNSHIP
PEDESTRIAN SAFETY AND ADA
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SITE PLAN



FAIRMOUNT AVENUE & SUNSET TERRACE



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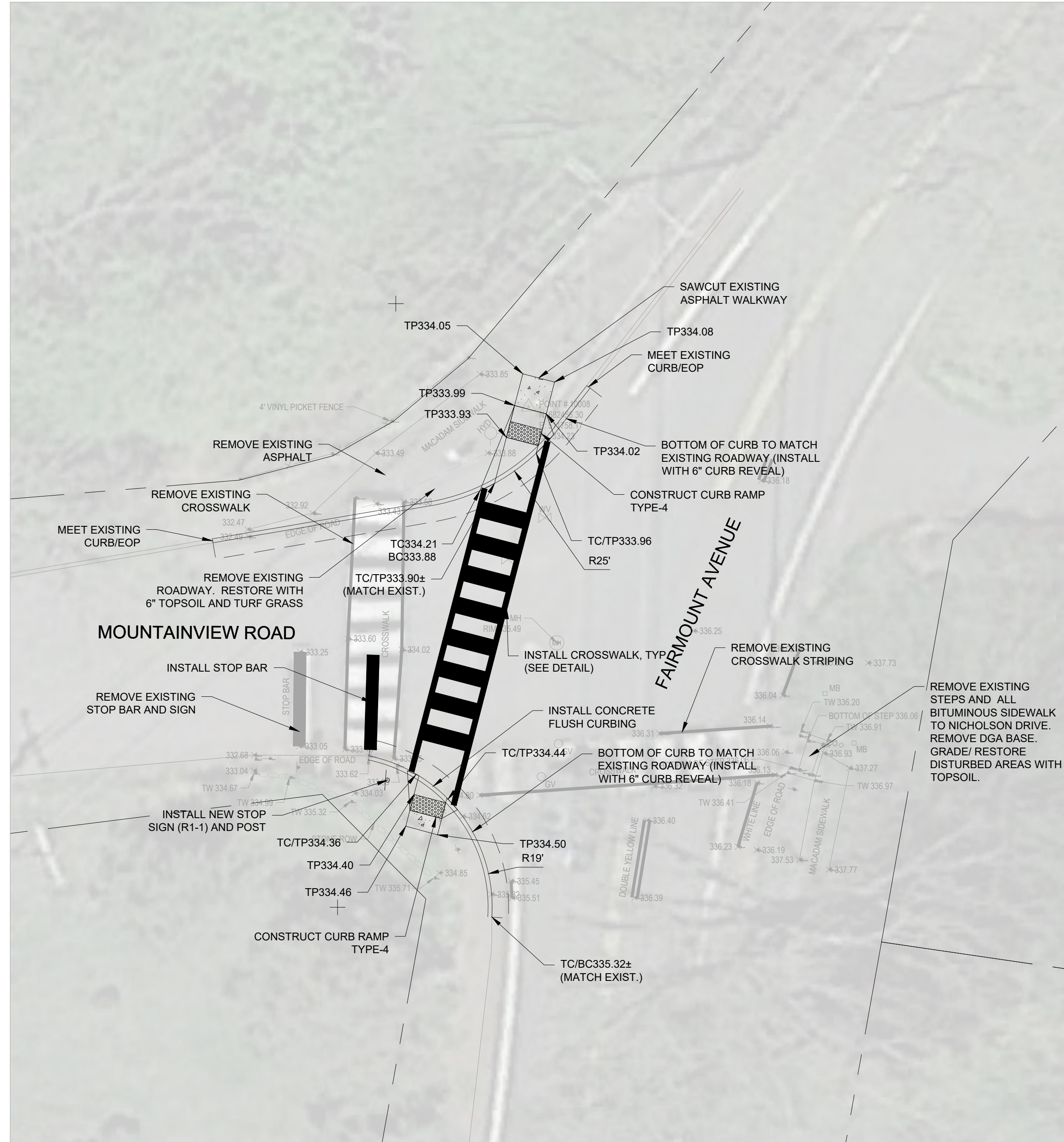
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Drawing Number			C-106		

Title
CHATHAM TOWNSHIP
PEDESTRIAN SAFETY AND ADA
IMPROVEMENTS

SITE PLAN



**CURB RAMP DETAIL
FAIRMOUNT AVENUE & MOUNTAINVIEW ROAD**



FAIRMOUNT AVENUE & NICHOLSON DRIVE



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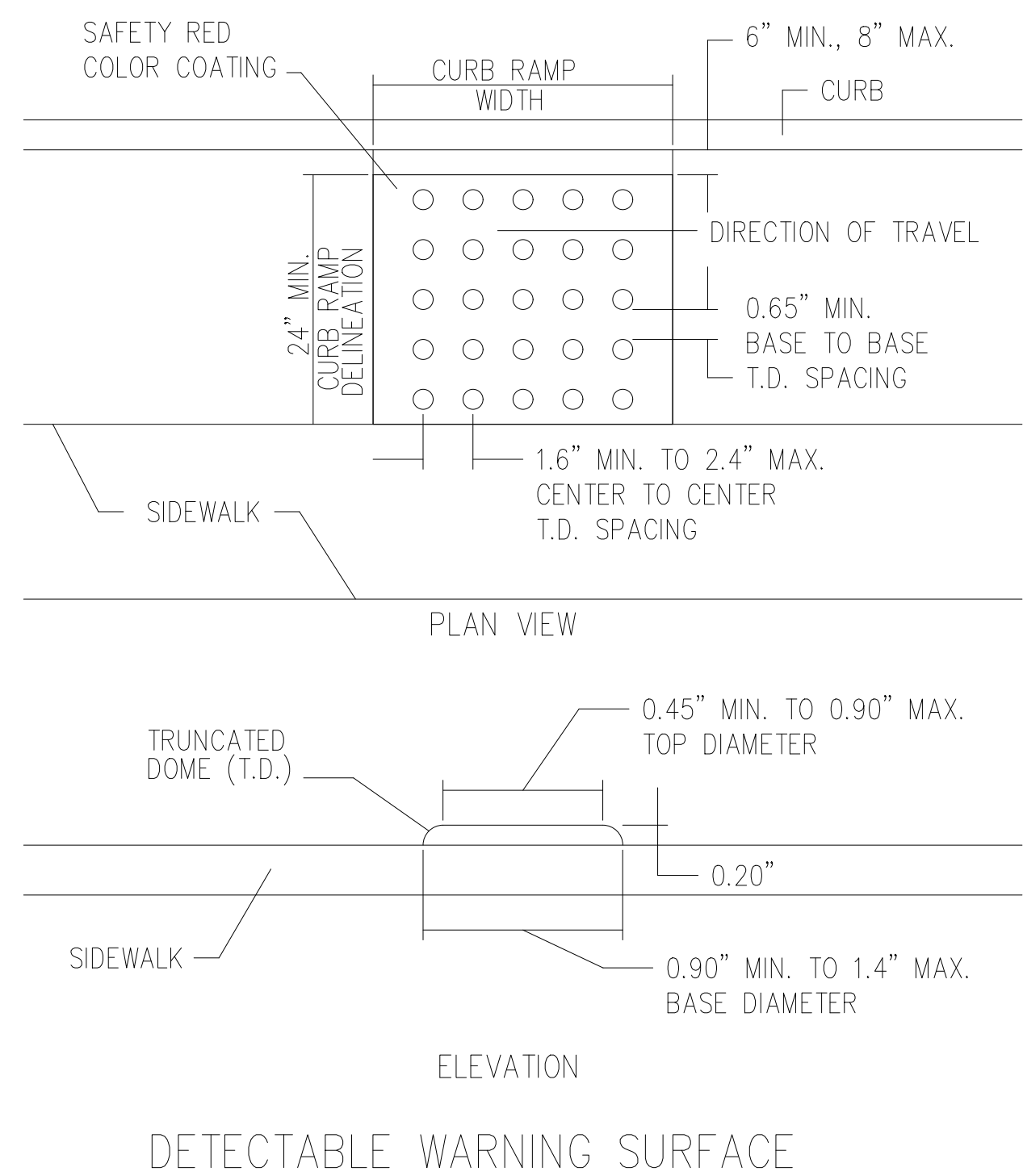
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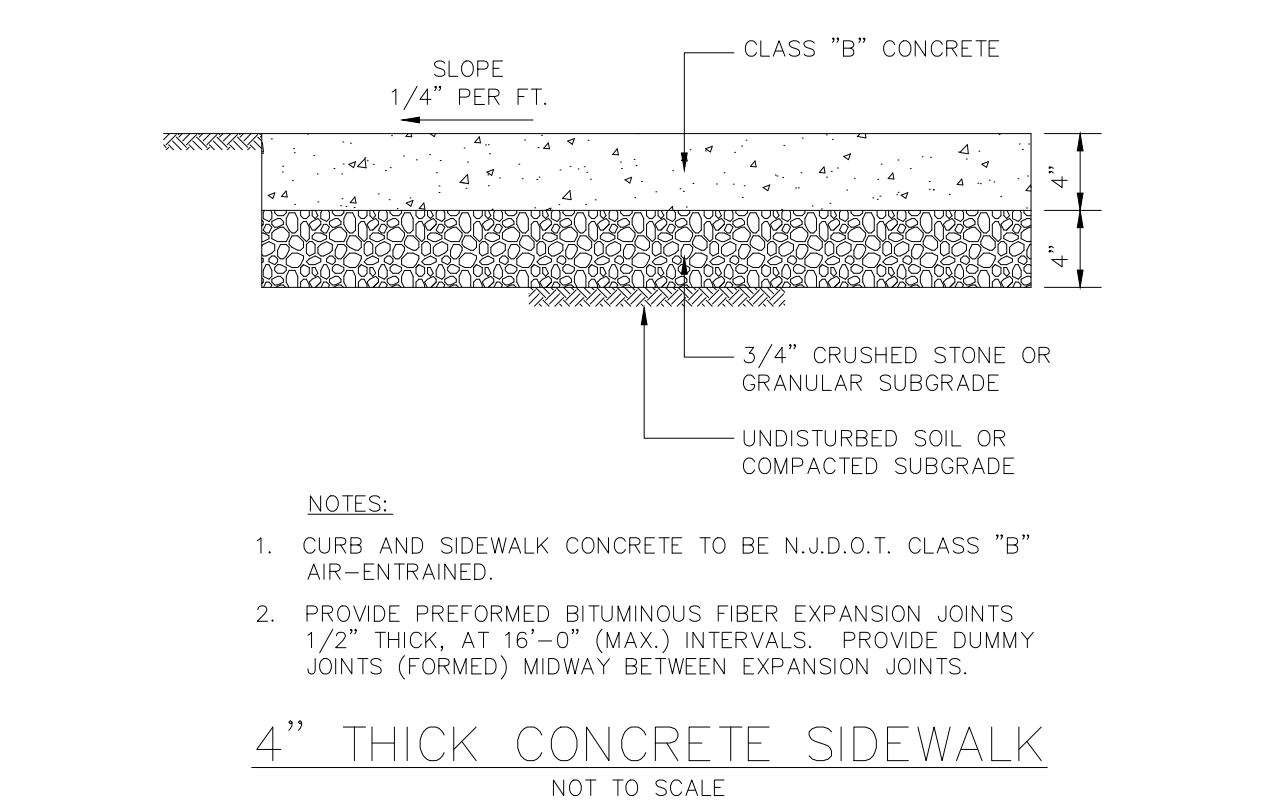
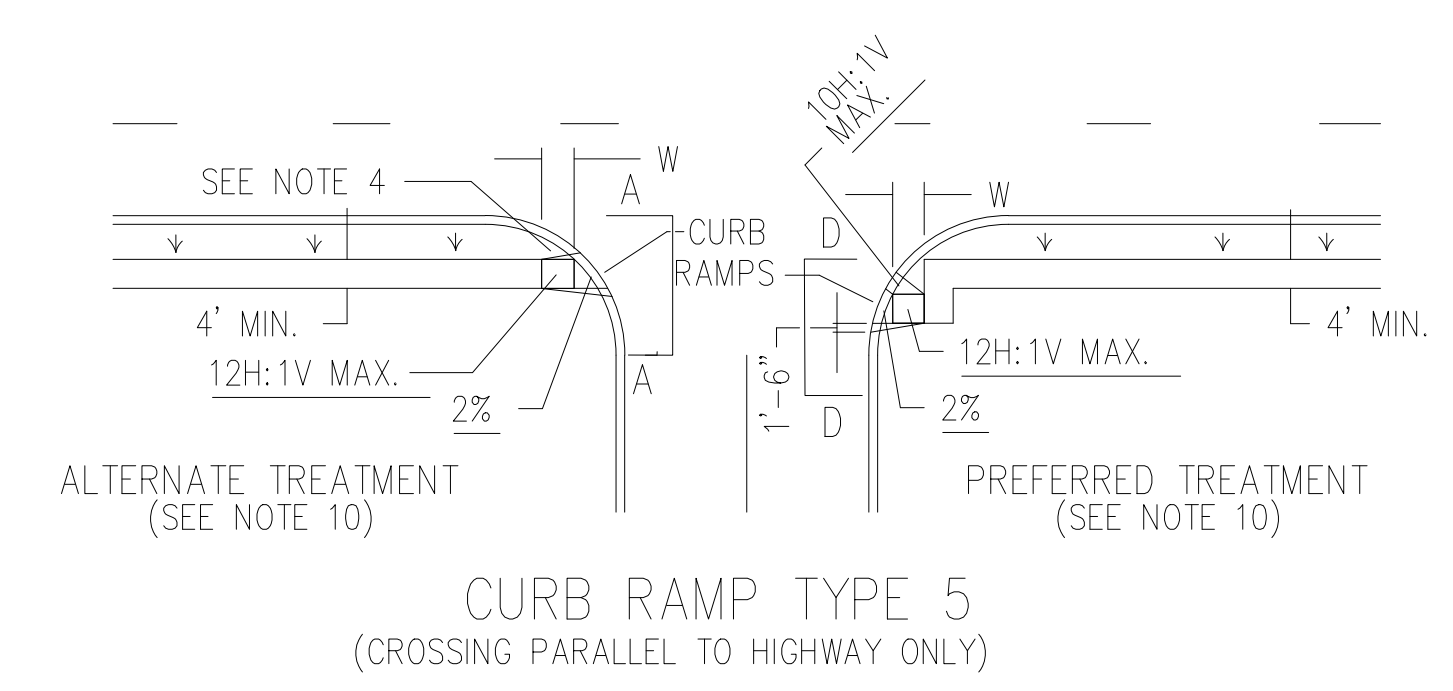
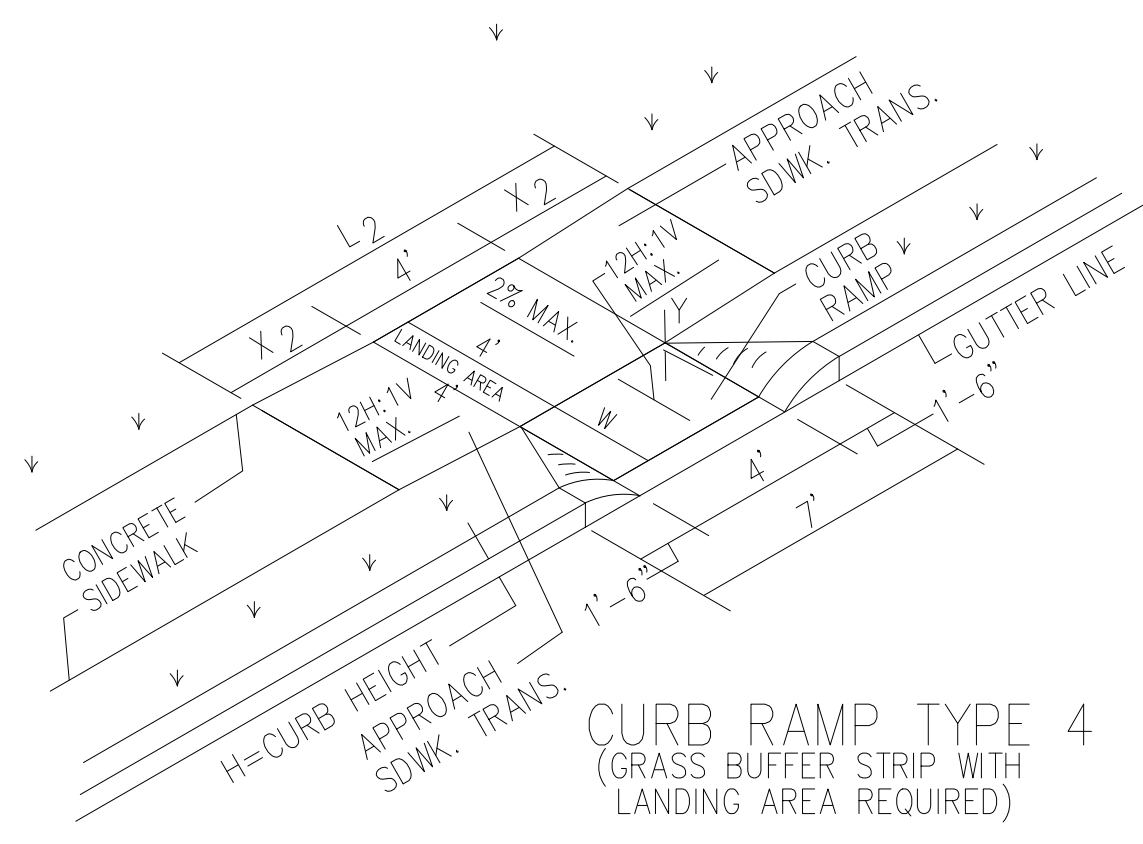
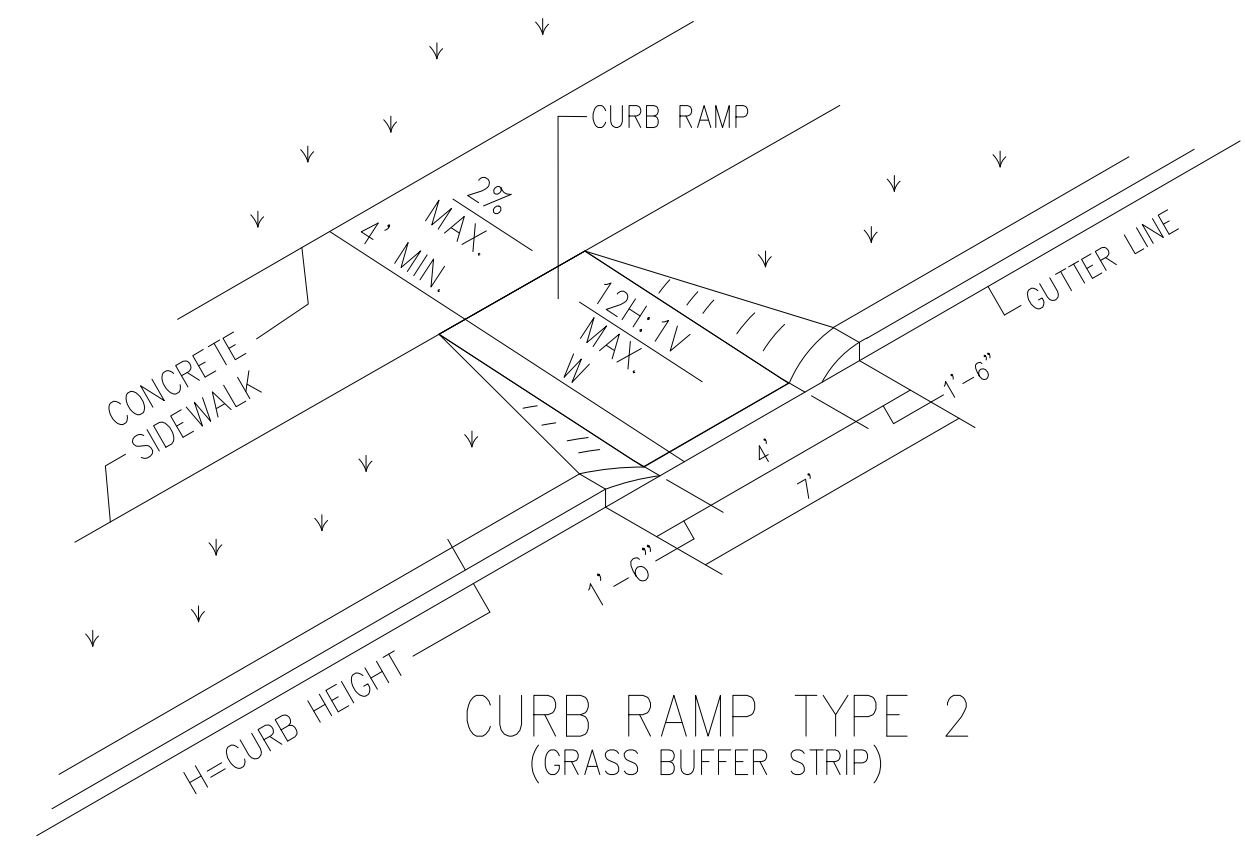
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Drawing Number	C-107				

Title
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PEDESTRIAN SAFETY AND ADA
IMPROVEMENTS**

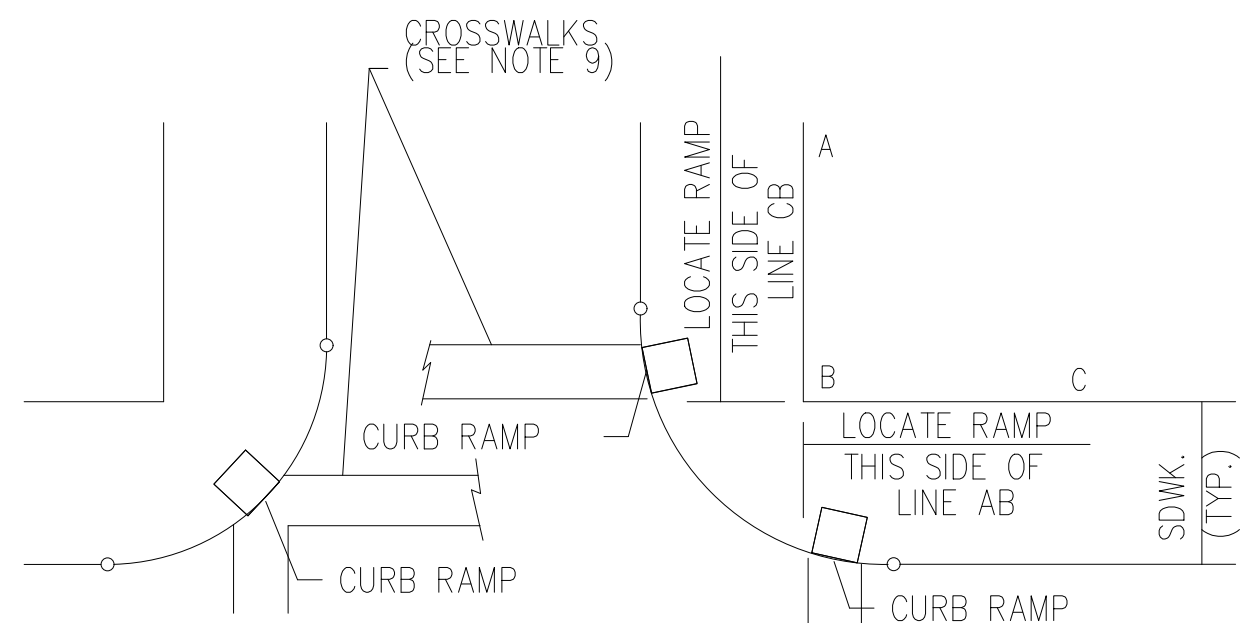
SITE PLAN



CD-607-1.1



- NOTES:
- CURB AND SIDEWALK CONCRETE TO BE N.J.D.O.T. CLASS "B" AIR-ENTRAINED.
 - PROVIDE PREFORMED BITUMINOUS FIBER EXPANSION JOINTS 1/2" THICK, AT 16'-0" (MAX.) INTERVALS. PROVIDE DUMMY JOINTS (FORMED) MIDWAY BETWEEN EXPANSION JOINTS.
- 4" THICK CONCRETE SIDEWALK
NOT TO SCALE

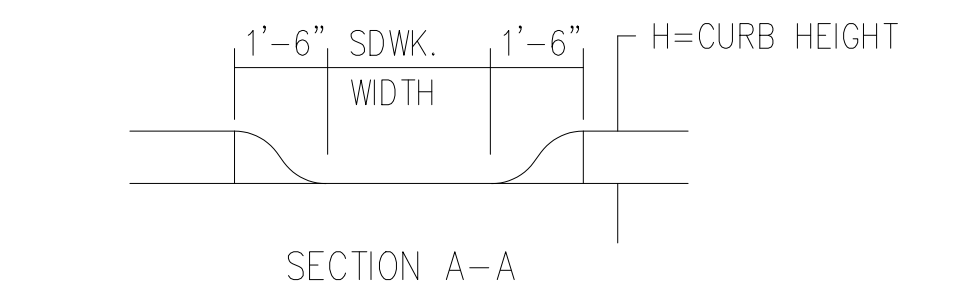


H INCHES	X1 FEET	L1 FEET	W FEET
3	2.5	9.0	3
4	3.3	10.6	4
5	4.2	12.4	5
6	5.0	14.0	6
7	5.8	15.6	7
8	6.7	17.4	8
9	7.5	19.0	9

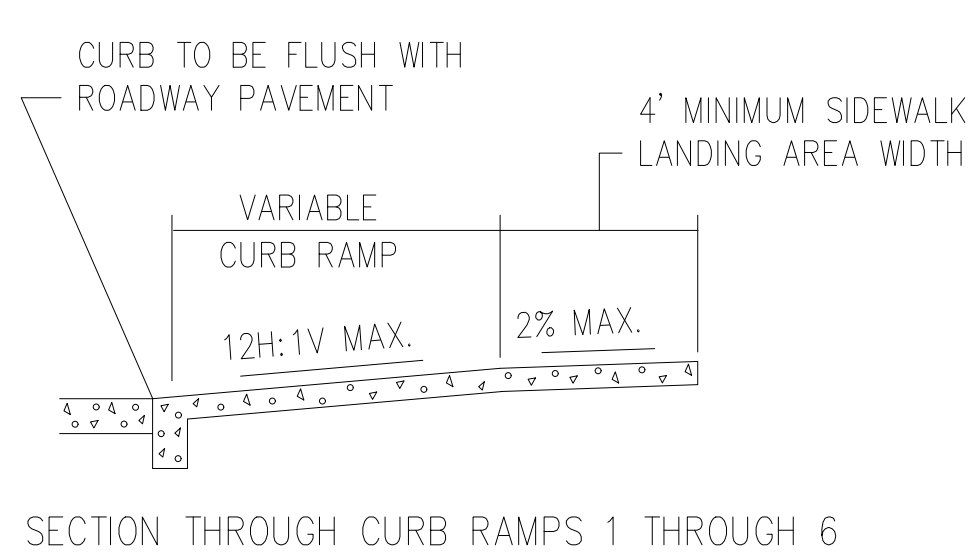
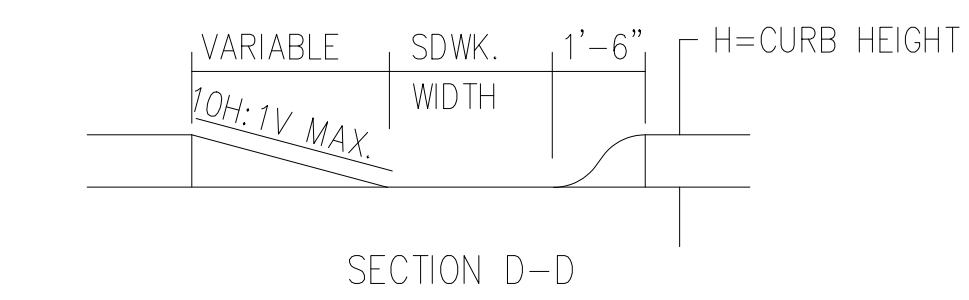
H INCHES	W FEET
3	3
4	4
5	5
6	6
7	7
8	8
9	9

ALTERNATE TREATMENT (SEE NOTE 10) PREFERRED TREATMENT (SEE NOTE 10)

LOCATION OF CURB RAMP TYPES 1, 2, 3, 4 & 7 FOR CROSSING PARALLEL AND PERPENDICULAR TO HIGHWAY



NOTE: CURB RAMP OPENING TO BE FLUSH WITH ROADWAY PAVEMENT (CURB RAMP TYPES 5 & 6).

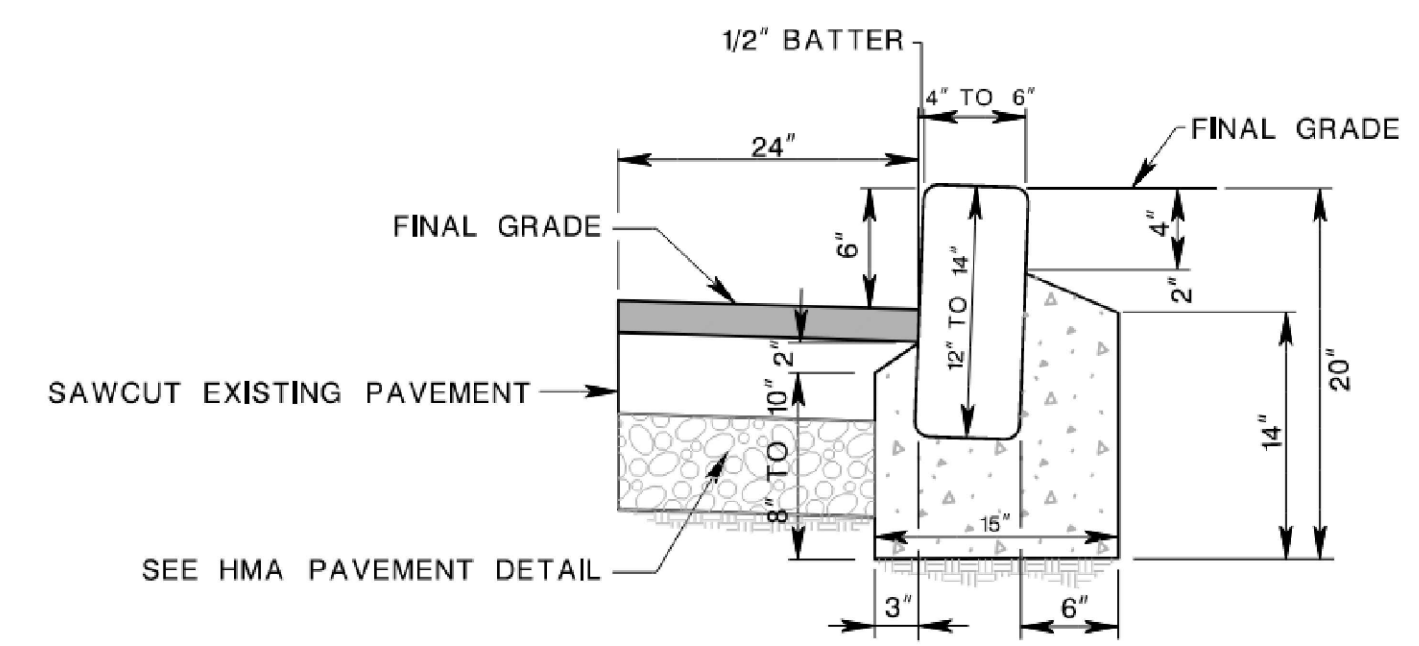


W FEET	H INCHES	X1 FEET	L1 FEET	Y INCHES	X2 FEET	L2 FEET
2.5	3	2.5	9	2.5	0.5	5
	4	3.3	10.6	2.5	1.5	7
	5	4.2	12.4	2.5	2.5	9
	6	5.0	14.0	2.5	3.5	11
	7	5.8	15.6	2.5	4.5	13
	8	6.7	17.4	2.5	5.5	15
	9	7.5	19.0	2.5	6.5	17
3.0	3	*	*	*	*	*
	4	3.3	10.6	3.0	1	6
	5	4.2	12.4	3.0	2	8
	6	5.0	14.0	3.0	3	10
	7	5.8	15.6	3.0	4	12
	8	6.7	17.4	3.0	5	14
	9	7.5	19.0	3.0	6	16
3.5	3	*	*	*	*	*
	4	3.3	10.6	3.5	0.5	5
	5	4.2	12.4	3.5	1.5	7
	6	5.0	14.0	3.5	2.5	9
	7	5.8	15.6	3.5	3.5	11
	8	6.7	17.4	3.5	4.5	13
	9	7.5	19.0	3.5	5.5	15
4.0	3	*	*	*	*	*
	4	*	*	*	*	*
	5	4.2	12.4	4.0	1	6
	6	5.0	14.0	4.0	2	8
	7	5.8	15.6	4.0	3	10
	8	6.7	17.4	4.0	4	12
	9	7.5	19.0	4.0	5	14

W FEET	H INCHES	Y INCHES	X2 FEET	L2 FEET
2.5	3	2.5	0.5	5
	4	2.5	1.5	7
	5	2.5	2.5	9
	6	2.5	3.5	11
	7	2.5	4.5	13
	8	2.5	5.5	15
	9	2.5	6.5	17
3.0	3	**	**	**
	4	3.0	1	6
	5	3.0	2	8
	6	3.0	3	10
	7	3.0	4	12
	8	3.0	5	14
	9	3.0	6	16
3.5	3	**	**	**
	4	3.5	0.5	5
	5	3.5	1.5	7
	6	3.5	2.5	9
	7	3.5	3.5	11
	8	3.5	4.5	13
	9	3.5	5.5	15
4.0	3	**	**	**
	4	**	**	**
	5	4.0	1	6
	6	4.0	2	8
	7	4.0	3	10
	8	4.0	4	12
	9	4.0	5	14

* TYPE 3 RAMP IS NOT APPLICABLE, USE TYPE 1.

** TYPE 4 RAMP IS NOT APPLICABLE, USE TYPE 2.



- NOTES:
- CONCRETE SHALL BE NJDOT CLASS "B" (AIR ENTRAINED)
 - FOUNDATION SHALL BE CONSTRUCTED THE ENTIRE LENGTH OF GRANITE BLOCK CURB.
 - BLOCK DIMENSIONS 4" X 7" X 12" WITH TOLERANCE OF 0.5" ON ALL DIMENSION
- GRANITE BLOCK CURB**
PER MORRIS COUNTY STANDARD
(NOT TO SCALE)

GENERAL NOTES:

- LANDING AREA, APPROACH SIDEWALK TRANSITIONS, AND CURB SHALL BE KEPT CLEAR OF OBSTRUCTIONS.
- DIMENSIONS SHOWN IN TABLES ARE FOR RELATIVELY FLAT SIDEWALK AREAS. CARE SHOULD BE TAKEN WHEN DETERMINING CURB RAMP SIZE BASED ON CURB HEIGHT (H) WHERE ELEVATION OF CURB AND SIDEWALK VARY DRASTICALLY IN AREA OF PROPOSED CURB RAMP.
- CURB (DROPPED CURB) GUTTERLINE TO BE FLUSH WITH ROADWAY PAVEMENT A MINIMUM OF 4 FEET AT ALL CURB RAMPS.
- FOR CURB RAMP TYPES 5 AND 6, IF A GRASS BUFFER DOES NOT EXIST, SLOPE CURB TO EQUAL SLOPE OF ADJACENT CURB RAMP.
- SIDEWALK AND CURB RAMP WITHIN AREA ENCLOSED BY HEAVY LINES TO BE PAID FOR AS CONCRETE SIDEWALK OF THE APPROPRIATE ADJACENT THICKNESS.
- CURB AND HEADER WITHIN AREA ENCLOSED BY HEAVY LINES TO BE PAID FOR AS VERTICAL CURB OR SLOPING CURB OF THE APPROPRIATE ADJACENT SIZE AND KIND.
- WHERE THE DISTANCE FROM THE GUTTER LINE TO THE OUTSIDE EDGE OF SIDEWALK IS 6 FEET OR LESS, CURB RAMP TYPE 7 SHOULD BE USED, INSTEAD OF CURB RAMP TYPE 1 THROUGH 4.
- THE PUBLIC SIDEWALK CURB RAMP, DETECTABLE WARNING SURFACE (SHADED AREA) SHALL BE SAFETY RED COLOR ON CONCRETE OR 70% COLOR CONTRAST FOR OTHER SURFACE SUCH AS BRICK.
- CROSSWALKS AND STOP LINES MAY BE MARKED OR UNMARKED. SEE PLANS.
- PREFERRED AND ALTERNATE TREATMENTS SHOULD NOT BE INTERMIXED WITHIN THE SAME INTERSECTION.
- DIMENSIONS SHOWN IN TABLES ARE FOR 3 INCH TO 9 INCH CURB HEIGHTS. WHERE THE CURB HEIGHTS ARE OTHER THAN WHAT IS PROVIDED IN THE TABLES, THE DIMENSIONS OF THE RAMPS WILL HAVE TO BE CALCULATED BASED ON CROSS SLOPES SHOWN.

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Client
TOWNSHIP OF CHATHAM MORRIS COUNTY, NEW JERSEY

Project Number
507393101-001

B/O
11

Total
19

JOHN K. RUSCHKE
PROFESSIONAL ENGINEER LIC. NO. 24GE0037148

John Ruschke

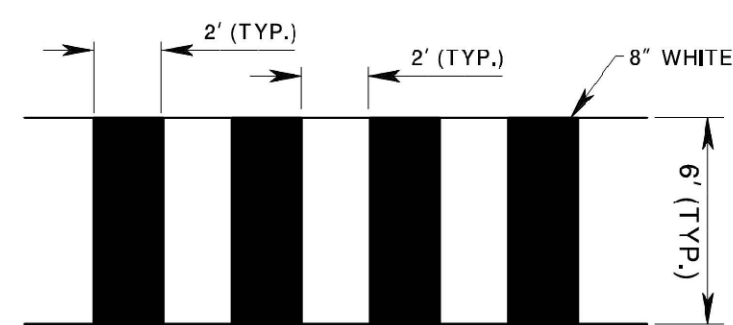
Date 08-03-2023

Designed	RS	05/23	Eng check	SJA	05/23
Drawn	RS	05/23	Coordination		
Dwg check	SJA	05/23	Approved	JKR	05/23
Scale at ARCH D	Status	Rev	Security		
N.T.S.	Draft	Rev0	STD		
Drawing Number	D-001				

Title
CHATHAM TOWNSHIP PEDESTRIAN SAFETY AND ADA IMPROVEMENTS

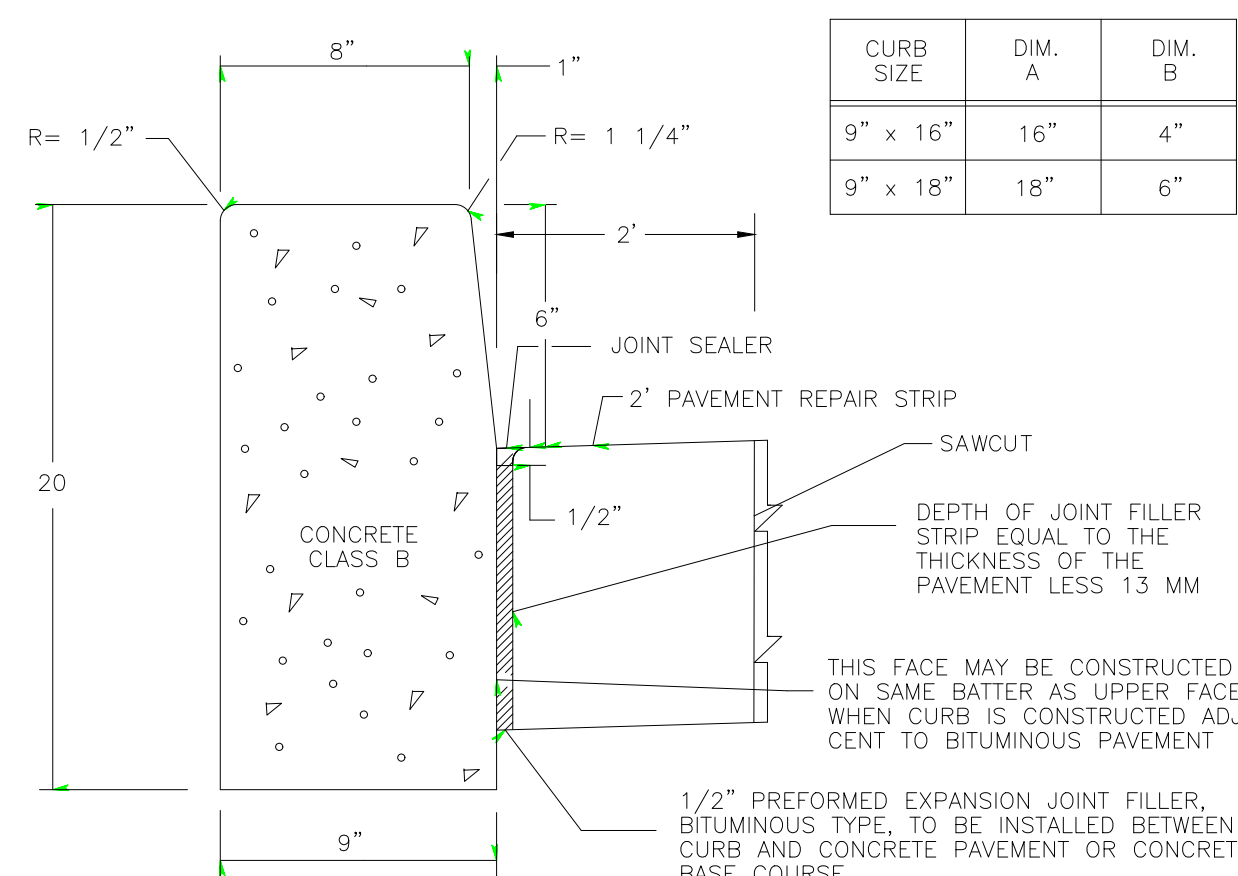
CONSTRUCTION DETAILS

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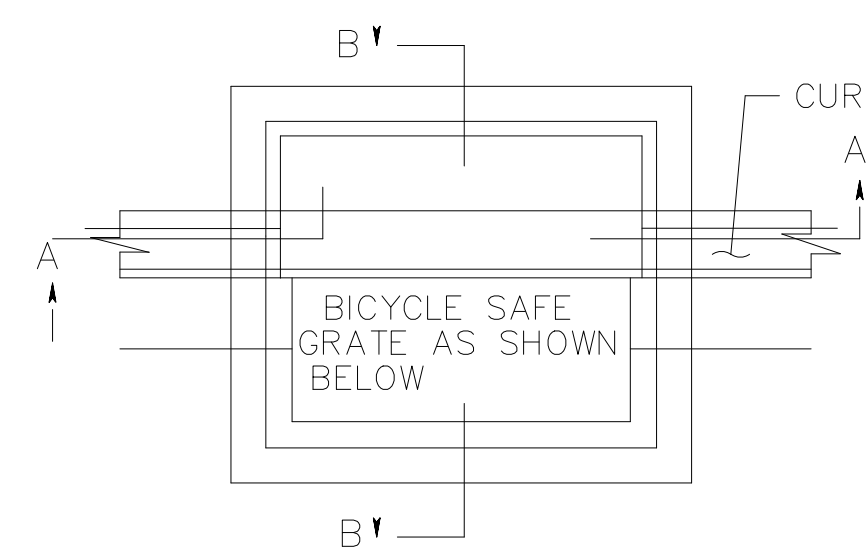
NOTE:
CROSSWALK MARKINGS SHALL BE ALIGNED PARALLEL TO THE DIRECTION OF VEHICULAR TRAFFIC.

TYPICAL CROSSWALK DETAIL
(NOT TO SCALE)



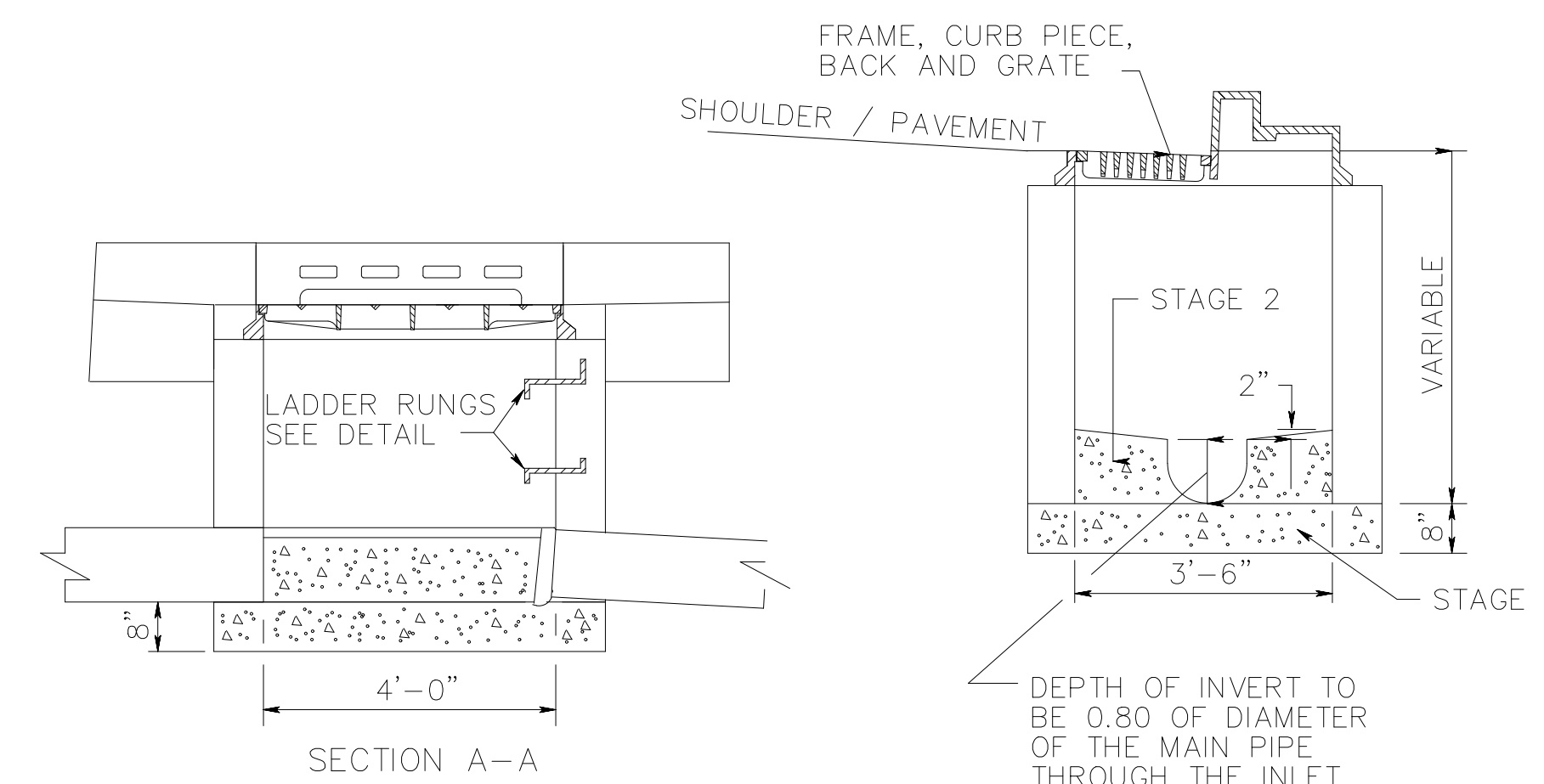
NOTES:
TRANSVERSE JOINTS 1/2" WIDE SHALL BE INSTALLED IN THE CURB 20 FT. APART AND SHALL BE FILLED WITH PREFORMED BITUMINOUS-IMPREGGATED FIBER JOINT FILLER RECESSED 1/4" IN FROM FRONT FACE AND TOP OF CURB.
EXPANSION JOINTS THRU AND ADJACENT TO THE CURB SHALL BE INCLUDED IN THE UNIT PRICE BID FOR CURB.

9"x20" CONCRETE VERTICAL CURB
N.T.S.



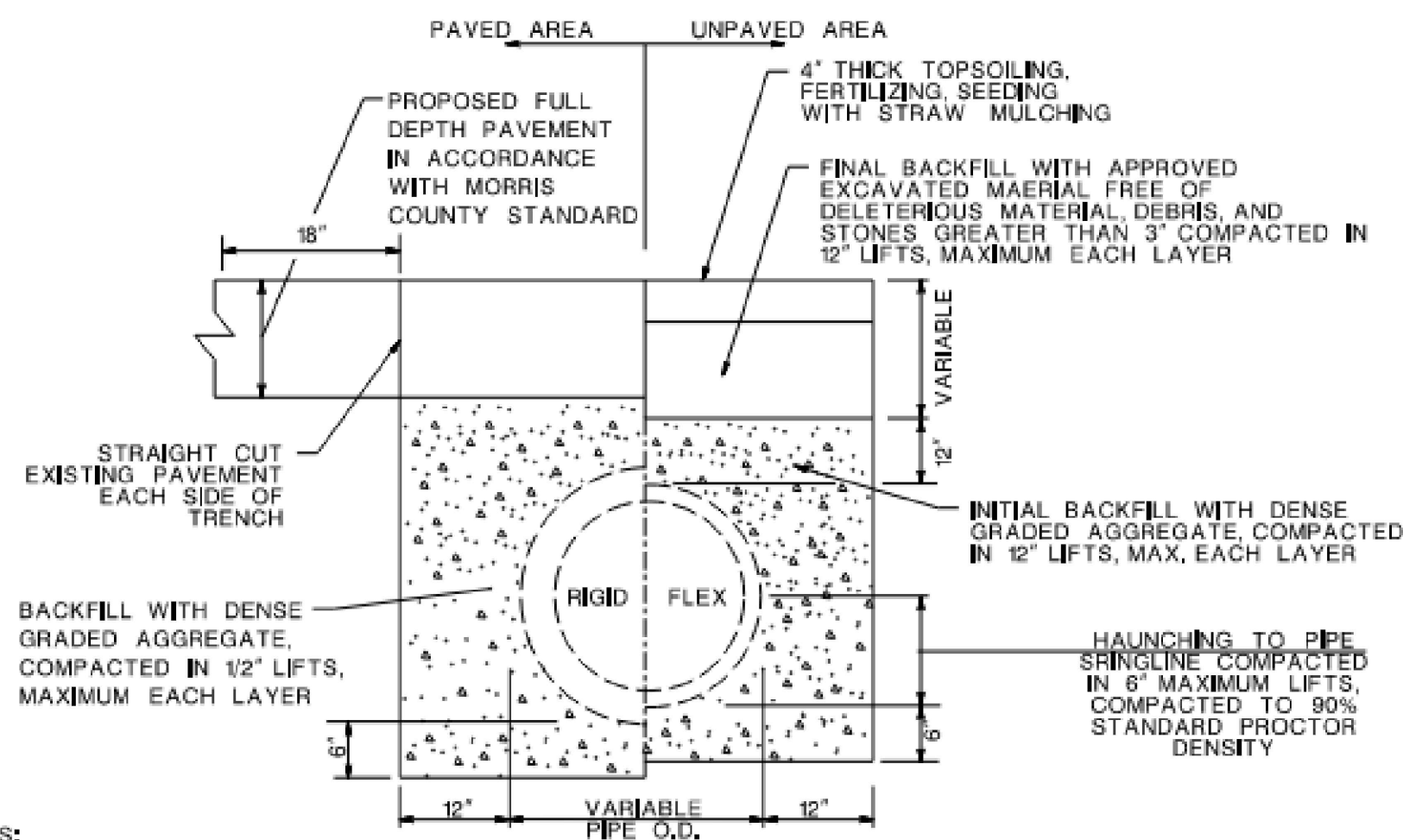
NOTE:

FOUNDATION AND INVERT TO BE CONSTRUCTED IN 2 STAGES. TOP SURFACE OF STAGE 1 TO BE LEFT ROUGH



INLET, TYPE B
NOT TO SCALE

SECTION B-B

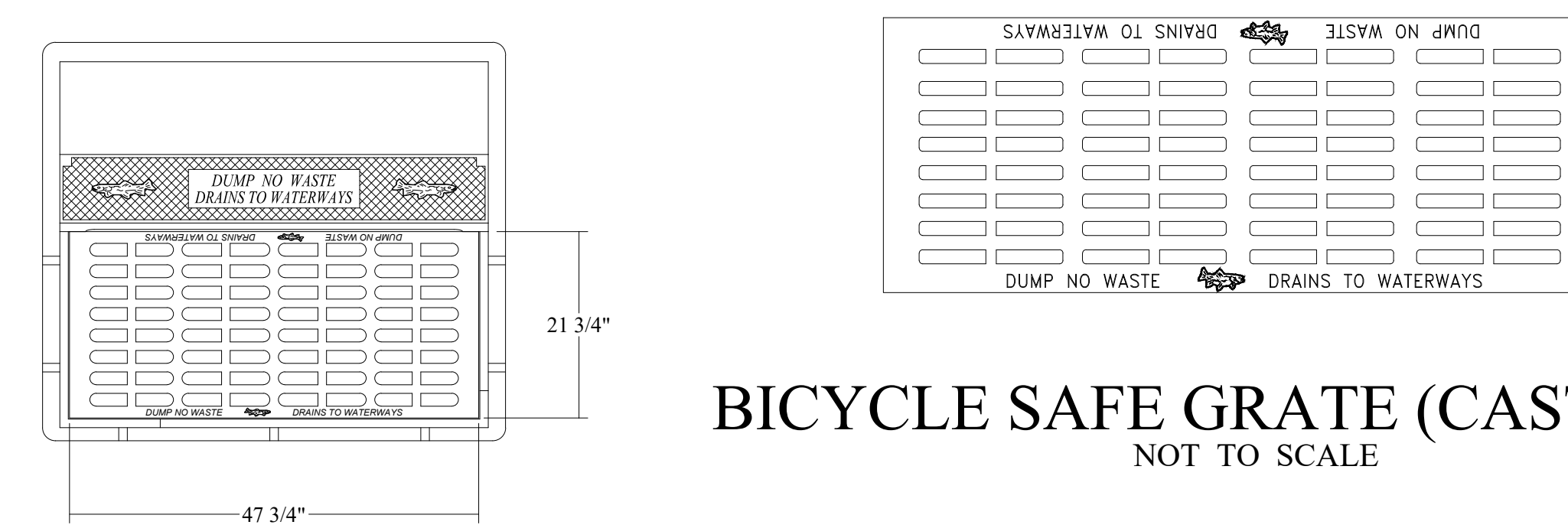


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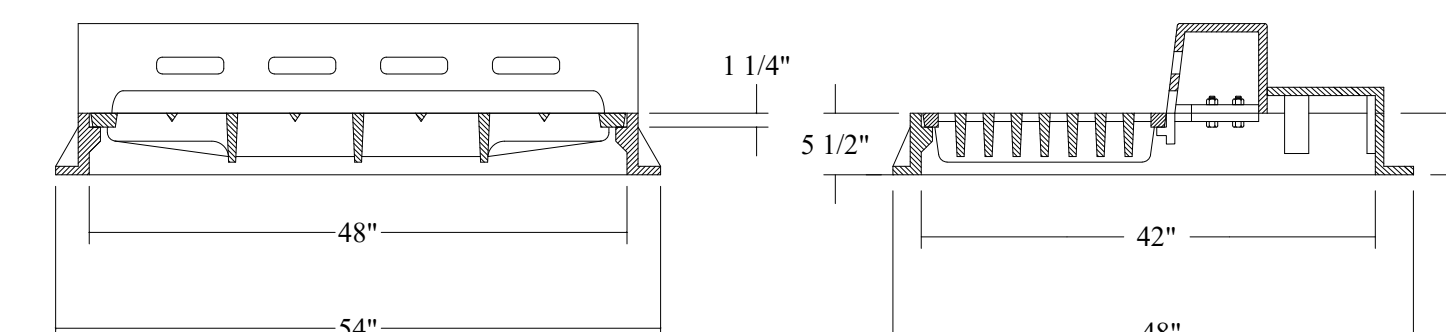
- REINFORCED CONCRETE CULVERT PIPE, MINIMUM 15" DIAMETER, SHALL BE UTILIZED IN ALL COUNTY RIGHT-OF-WAYS WHETHER PAVED OR UNPAVED.
- CONTRACTOR SHALL HAND COMPACT DGA BENEATH THE HAUNCHES OF FLEXIBLE PIPE TO ENSURE INSTALLATION MEETS OR EXCEEDS MANUFACTURERS RECOMMENDATION.
- IF NO OTHER CONTROLLING FACTORS EXIST, ALL CONCRETE PIPE SHALL BE INSTALLED IN ACCORDANCE WITH ACTIVE STANDARD ASTM C1479, ALL THERMOPLASTIC PIPE SHALL BE INSTALLED IN ACCORDANCE WITH ACTIVE STANDARD ASTM D2321.
- SOIL IN THE OUTER BEDDING, HAUNCH, AND LOWER SIDE ZONES SHALL BE COMPACTED TO AT LEAST THE SAME COMPACTION AS THE MAJORITY OF SOIL IN THE OVERFILL ZONE. SOIL SHALL BE COMPACTED WITHIN 2% OF OPTIMUM WATER CONTENT.
- BACKFILLING WITH EXCAVATED MATERIAL SHALL NOT BE PERMITTED IN TRENCHES FOR STORM SEWERS AND ALL OTHER UTILITIES.
- TYPE OF SEEDING SHALL MEET THE MORRIS COUNTY SOIL CONSERVATION DISTRICT REQUIREMENTS.
- SURFACE COURSE PAVEMENT SHALL BE PLACED IN TWO STAGES IN ACCORDANCE WITH THE FOLLOWING:
 - A. STAGE 1 - 2" HMA 9.5M64 SHALL BE COMPACTED FLUSH TO EXISTING PAVEMENT.
 - B. STAGE 2 - AFTER 6 MONTHS, THE TRENCH WIDTH PLUS 36" SHALL BE MILLED 2" DEEP AND SHALL BE RESURFACED WITH 2" HMA 9.5M64 COMPACTED FLUSH TO EXISTING PAVEMENT.

STANDARD TRENCH

PER MORRIS COUNTY STANDARD
NOT TO SCALE



BICYCLE SAFE GRATE (CAST IRON)
NOT TO SCALE



TYPE 'B' INLET CASTING
NOT TO SCALE

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**TOWNSHIP OF CHATHAM
MORRIS COUNTY, NEW JERSEY**

JOHN K. RUSCHKE
PROFESSIONAL ENGINEER LIC. NO. 24GE0037148

John Ruschke
Date 08-03-2023

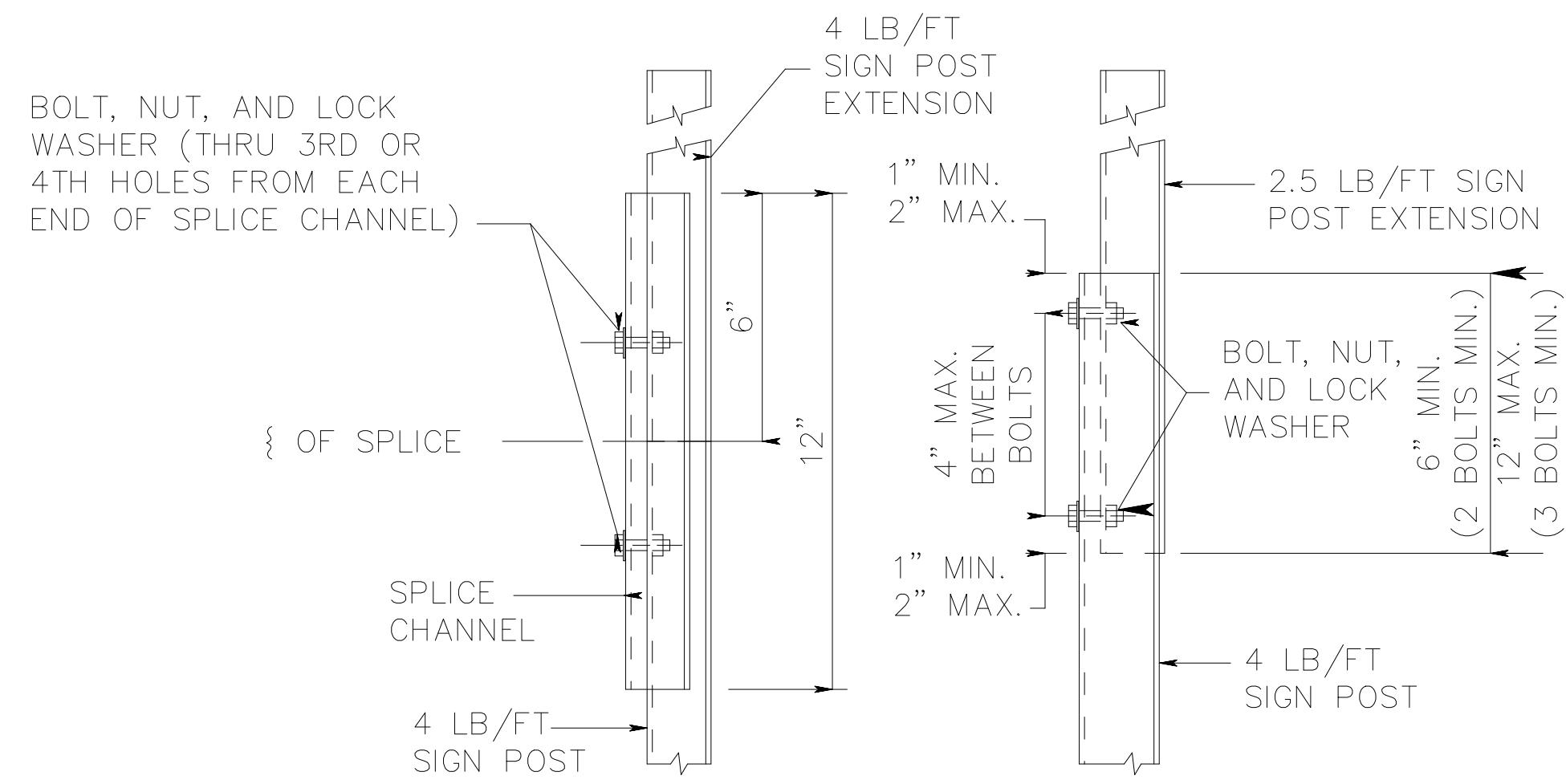
Rev	Date	Drawn	Description	Ch'k'd	App'd

Project Number	507393101-001	B/O	12	Total	19
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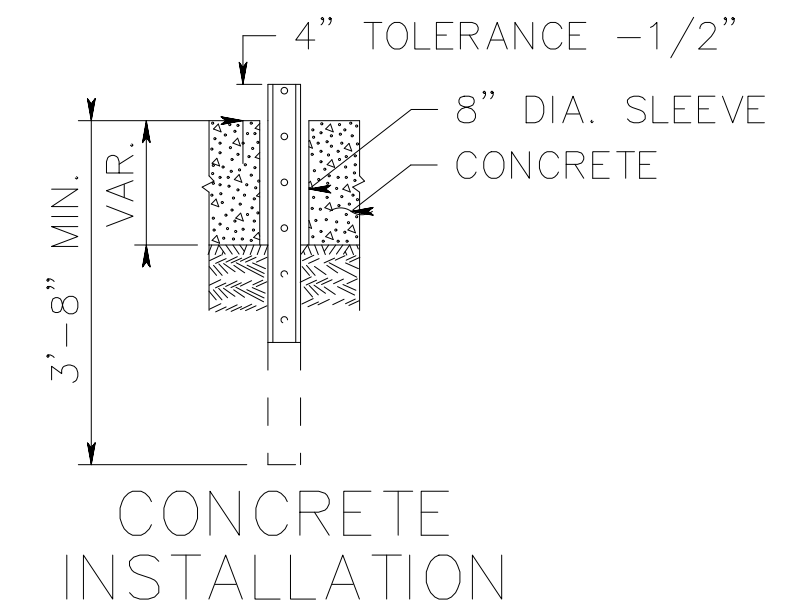
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Scale at ARCH D	Status	Rev	Rev	Security	
N.T.S.	Draft	Rev0	Rev0	STD	
Drawing Number		D-002			

Title
**CHATHAM TOWNSHIP
PEDESTRIAN SAFETY AND ADA
IMPROVEMENTS**

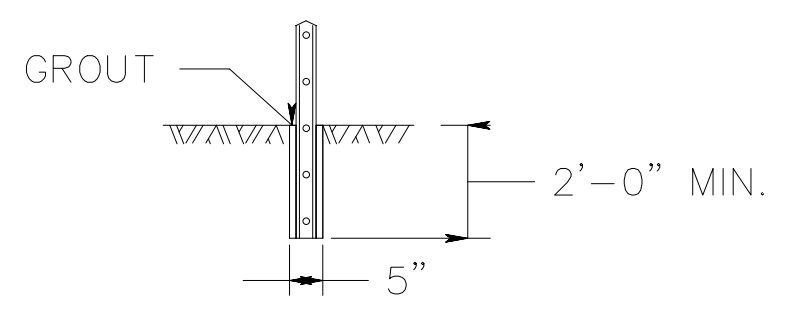
CONSTRUCTION DETAILS



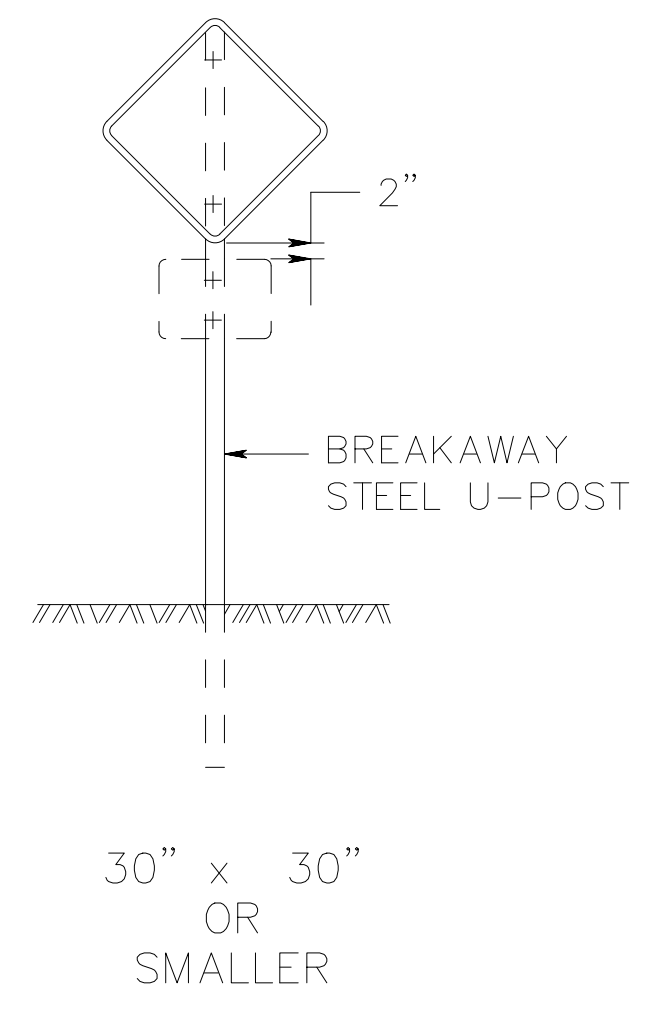
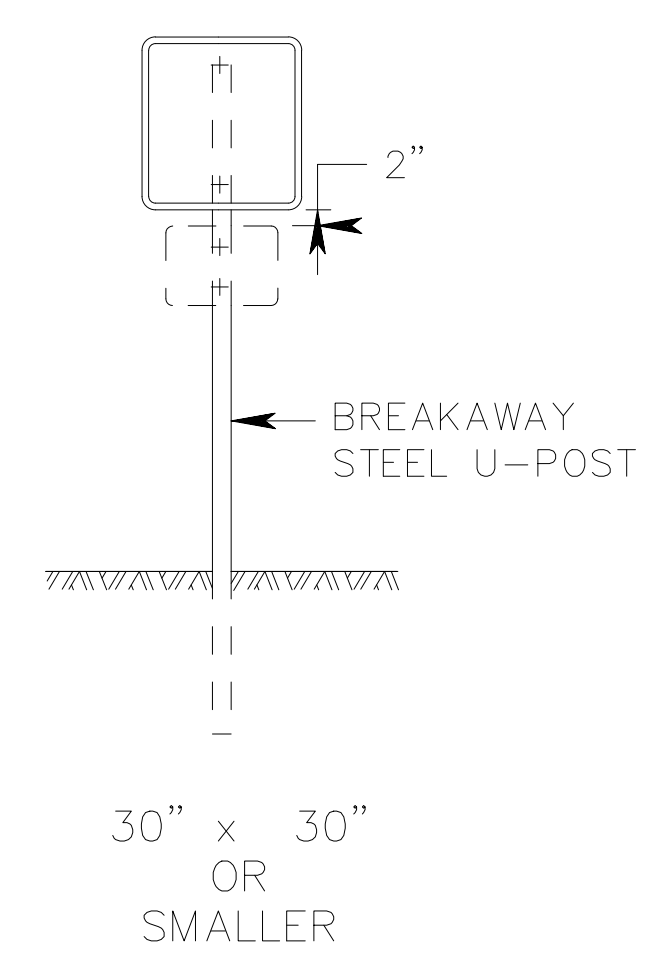
SIGN POST EXTENSION SPLICE DETAILS



CONCRETE INSTALLATION



ROCK INSTALLATION

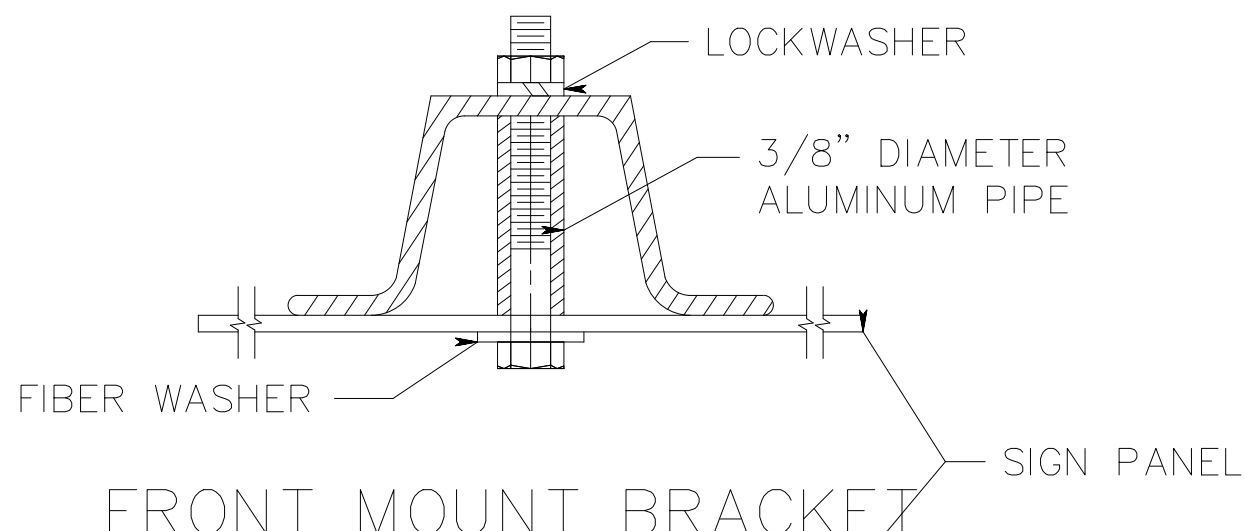


STEEL U-POST SIGN SUPPORTS

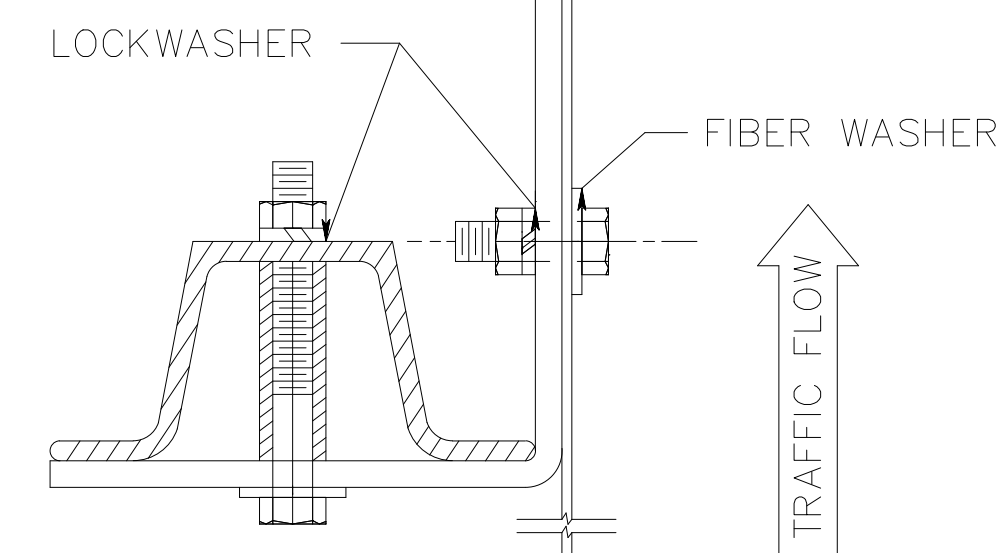
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GENERAL NOTES:

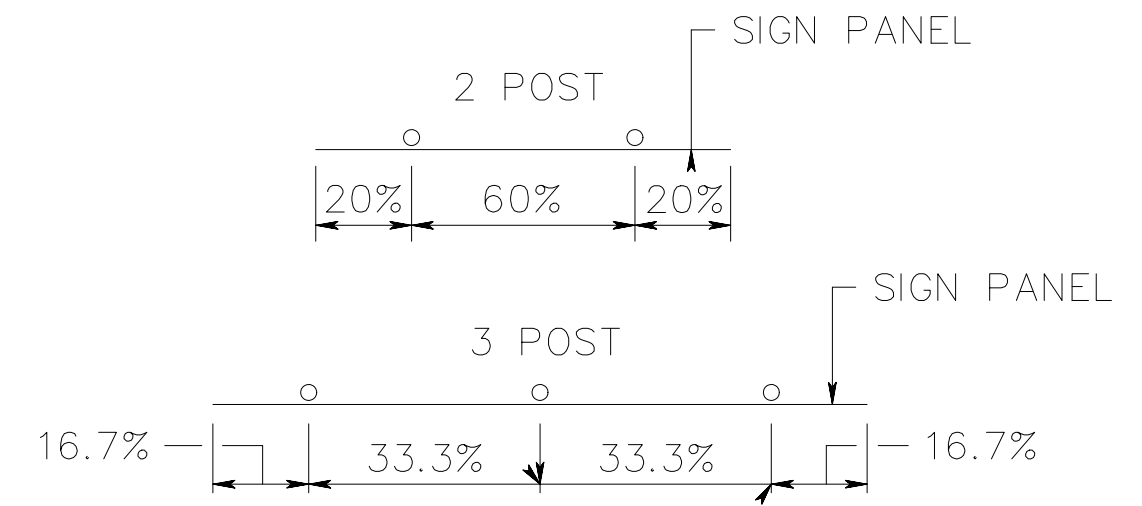
- ALL POSTS SHALL BE OF ADEQUATE LENGTH TO MEET THE REQUIREMENTS FOR ERECTION AS STATED IN THE CURRENT "MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES FOR STREETS AND HIGHWAYS" AND AS INDICATED BELOW.
- ALL SMALL SIGN SUPPORTS SHALL BE OF THE BREAKAWAY TYPE WITH EXCEPTION OF THOSE INSTALLED BEHIND GUIDE RAIL OR OTHER ROADSIDE BARRIER.
- ALL STEEL POSTS AND BRACKETS SHALL BE CUT, BENT, AND HOLES PUNCHED AND DRILLED BEFORE GALVANIZING. GALVANIZING SHALL BE IN CONFORMANCE WITH ASTM A123.
- ALL STEEL U-POST SIGN SUPPORTS MUST BE INSTALLED FACING THE PREDOMINANT TRAFFIC FLOW. A MOUNTING BRACKET SHOULD BE USED ON SIDE MOUNTED SIGNS SUCH AS "ONE WAY" SIGNS INSTALLED IN MEDIANS.
- SIGN PANEL SIZES SHALL DETERMINE POST TYPE AND NUMBER AS SHOWN ON THIS DETAIL.
- BOLTS SHALL NOT PROTRUDE MORE THAN 3/4" BEYOND THE NUT WHEN TIGHT, BUT SHALL ENGAGE ALL THREADS IN THE NUT.
- WHEN SIGNS ARE INSTALLED ON SLOPES 10H:1V OR FLATTER, THE MINIMUM VERTICAL CLEARANCE REQUIREMENTS FOR SIGNS ARE:
 FOR SINGLE POST INSTALLATIONS - THE MINIMUM DISTANCE BETWEEN THE EDGE OF THE PAVEMENT AND THE BOTTOM OF ANY PANEL MUST BE 7 FEET, AND THE MINIMUM DISTANCE FROM EDGE OF PAVEMENT TO THE TOP OF ANY SIGN PANEL MUST BE 9 FEET.
 FOR MULTI-POST INSTALLATIONS - THE MINIMUM DISTANCE BETWEEN THE EDGE OF PAVEMENT AND THE BOTTOM OF A MAJOR SIGN PANEL MUST BE 7 FEET.
 SECONDARY SIGN PANELS (LAND SERVICE HIGHWAYS) - THE MINIMUM DISTANCE BETWEEN THE EDGE OF PAVEMENT AND THE BOTTOM OF A SECONDARY SIGN PANEL IS 6 FEET.
 SECONDARY SIGN PANELS (INTERSTATE AND FREEWAYS) - THE BOTTOM OF THE MAJOR SIGN SHALL BE A MINIMUM OF 8 FEET AND THE SECONDARY SIGN PANEL A MINIMUM OF 5 FEET ABOVE THE EDGE OF PAVEMENT.
 WHERE GRADING OF 10H:1V OR FLATTER CANNOT BE OBTAINED, OR WHERE CURB OR BERM IS GREATER THAN 4 INCHES, THE MINIMUM VERTICAL CLEARANCE WILL BE MEASURED FROM THE GROUND LINE TO THE BOTTOM OF THE SIGN.
- PERMANENT SIGN SUPPORTS SHOULD NOT BE INSTALLED ON SLOPES GREATER THAN 10H:1V, EXCEPT WHERE GRADING OF 10H:1V CANNOT BE OBTAINED OR THE SIGN SUPPORTS WILL BE BEHIND A TRAFFIC BARRIER. THE SLOPE SHALL EXTEND A MINIMUM OF 3 FEET BEYOND THE OUTSIDE EDGE OF SIGN (SEE GRADING DETAIL FOR SLOPE TREATMENT).
- EXTRUDED ALUMINUM SIGN PANELS ARE NOT PERMITTED FOR USE WITH STEEL U-POST SIGN SUPPORTS.
- STEEL U-POST SIGN SUPPORTS SHALL NOT BE PLACED IN FRONT OF GUIDE RAIL AND THE POSTS MUST NOT STRADDLE GUIDE RAIL.
- TO EXTEND THE HEIGHT OF A SIGN POST, A MAXIMUM OF ONE SPLICE MAY BE MADE AND MUST BE A MINIMUM OF 9 FEET FROM THE GROUNDLINE TO CENTER LINE OF SPLICE.



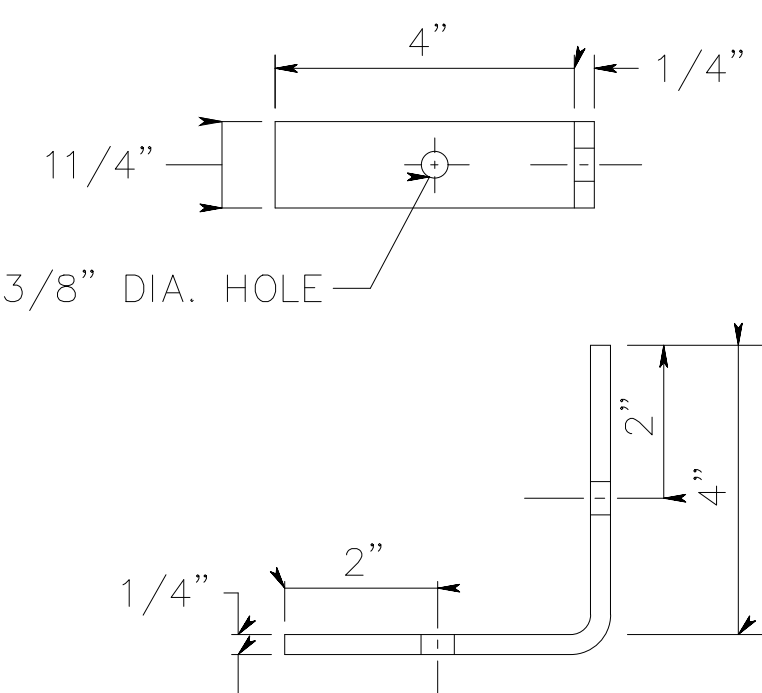
FRONT MOUNT BRACKET



SIDE MOUNT BRACKET



STEEL U-POST SPACING



DETAIL OF BRACKET FOR SIDE MOUNTED SIGNS

PANEL SIZE (W x H)	# OF POSTS	POST SIZE (LB/ FT)
18" x 18"	1	2.5
18" x 24"	1	2.5
24" x 24"	1	2.5
24" x 30"	1	2.5
24" x 36"	1	2.5
30" x 24"	1	2.5
30" x 30"	1	2.5
36" x 12"	2	2.5
36" x 36" x 36"	2	2.5
30" x 36"	1	4.0

PANEL SIZE (W x H)	# OF POSTS	POST SIZE (LB/ FT)
36" x 36"	2	2.5
36" x 48"	2	2.5
45" x 36"	2	2.5
48" x 24"	2	2.5
48" x 36"	2	2.5
48" x 48"	2	4.0
48" x 64" x 64"	2	2.5
60" x 36"	2	4.0
48" x 60"	2	4.0
60" x 30"	2	4.0

U-POST SELECTION TABLE BREAKAWAY SIGN SUPPORT

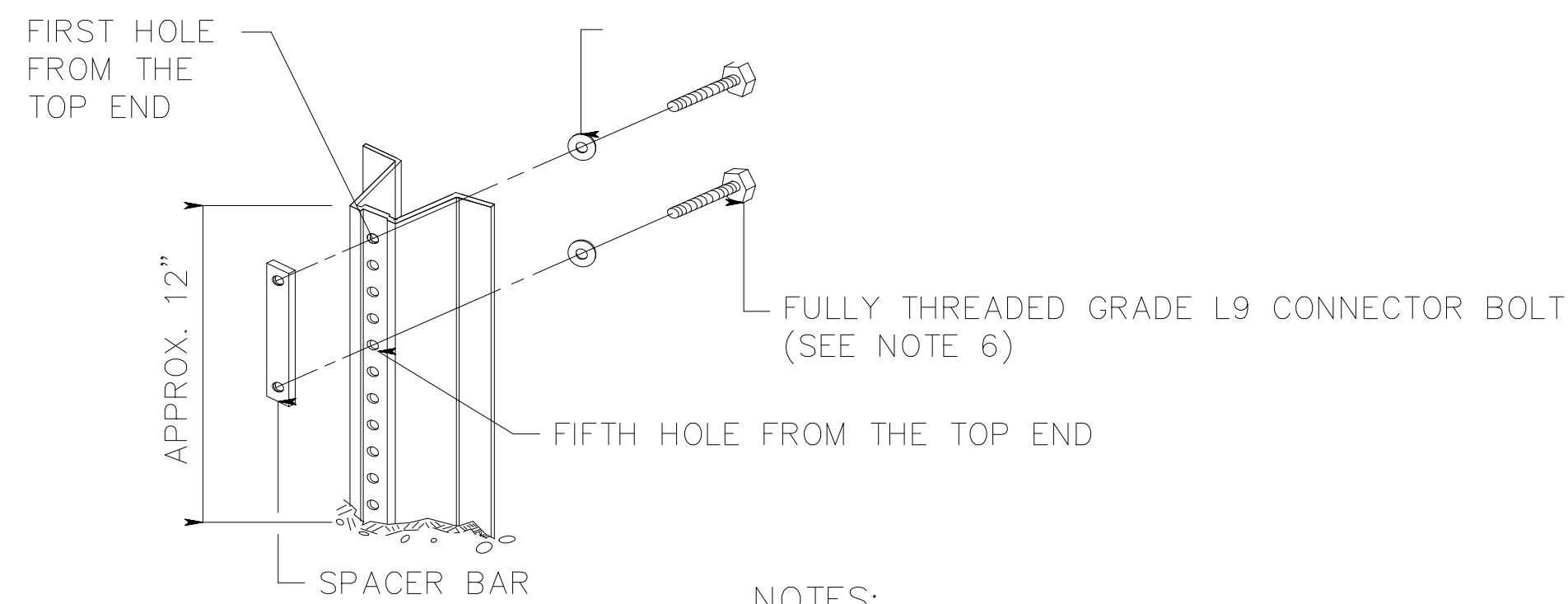
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					<p>Drawn</p> <p>RS 05/23</p>	<p>Coordination</p>		<p>Approved</p> <p>JKR 05/23</p>	<p>Drawing Number</p> <p>D-003</p>		

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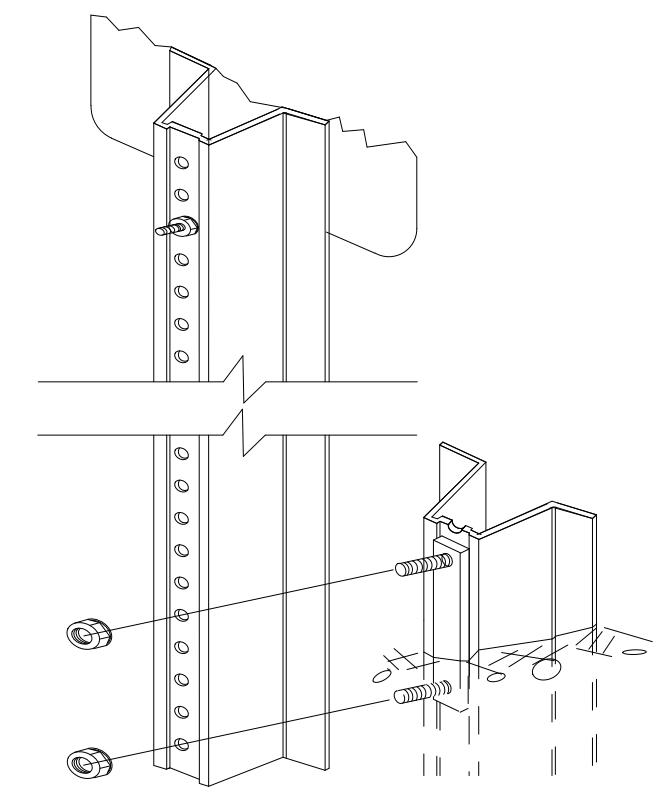
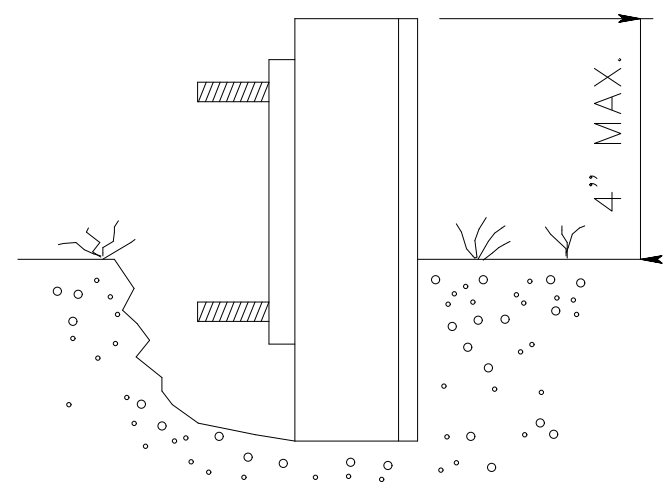
John Ruschke
 Date 08-03-2023

Rev	Date	Drawn	Description	Ch'k'd	App'd

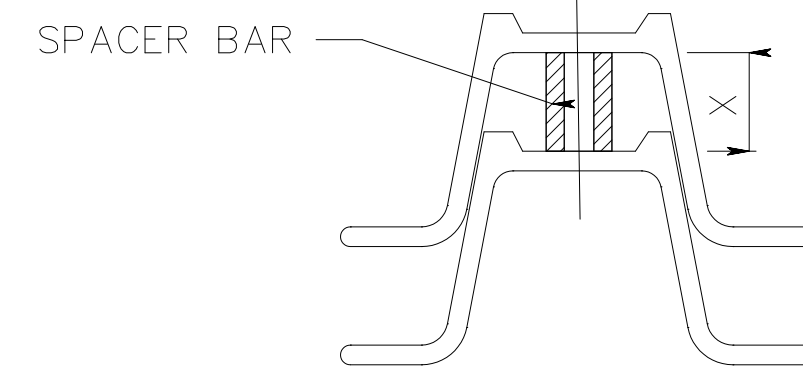
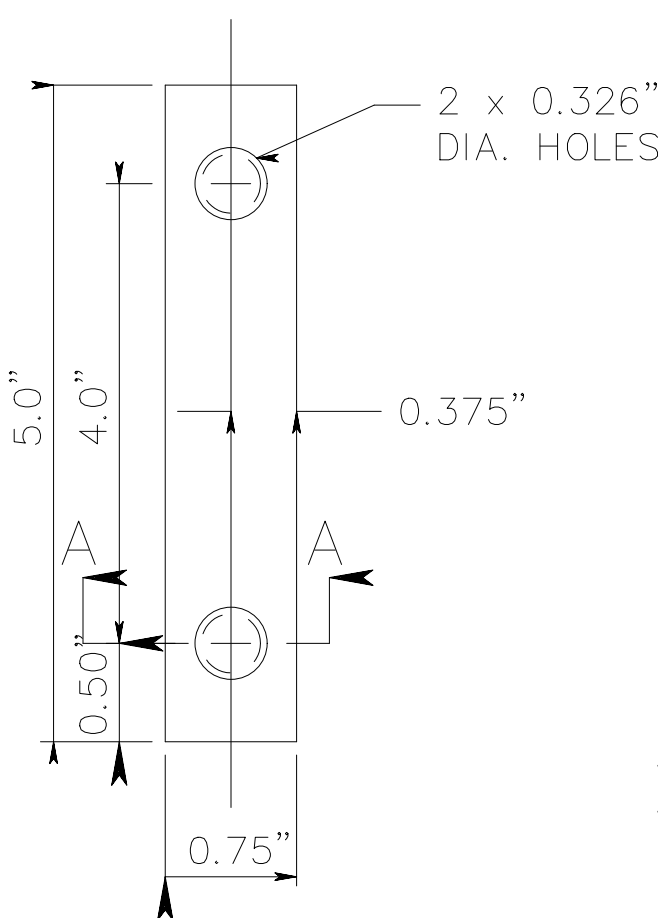


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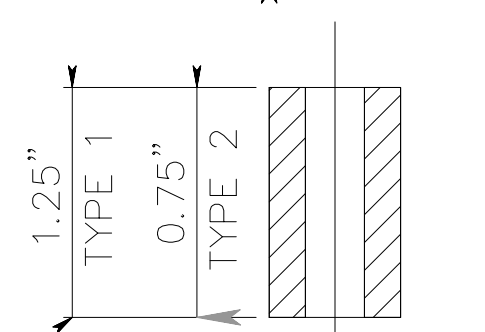
1. DRIVE ANCHOR POST ASSEMBLY TO WITHIN APPROXIMATELY 12 INCHES ABOVE GROUND LEVEL. PLACE BOLT AND WASHER IN FIRST AND FIFTH HOLES FROM THE TOP END, AND SECURE BOLTS ONTO SPACER.
2. DRIVE ANCHOR POST ASSEMBLY TO WITHIN A MAXIMUM OF 4 INCHES ABOVE GROUND LEVEL.
3. DIG OUT AROUND BACK OF ANCHOR POST ASSEMBLY TO ALLOW ROOM FOR TOP POST TO BE ATTACHED.
4. NEST TOP POST ASSEMBLY ONTO PROTRUDING ANCHOR POST ASSEMBLY BOLTS, THROUGH THE FIRST AND FIFTH HOLES FROM THE BOTTOM OF THE TOP POST.
5. PLACE AND TIGHTEN A SELF-LOCKING FLANGE NUT ON EACH BOLT. WHEN INSTALLATION IS COMPLETE, TOP OF GROUND POST SHALL NOT EXCEED 4 INCHES ABOVE GROUND LEVEL.
6. SIZE OF CONNECTOR BOLT FOR TYPE 1, 5/16" x 11/2"
SIZE OF CONNECTOR BOLT FOR TYPE 2, 5/16" x 2"
7. THE CONNECTOR BOLTS SHALL BE FULLY THREADED. EACH CONNECTOR BOLT AND NUT SHALL BE CLEARLY STAMPED WITH MANUFACTURER'S IDENTIFYING MARK.



ANCHOR POST ASSEMBLY
SIGN SUPPORTS



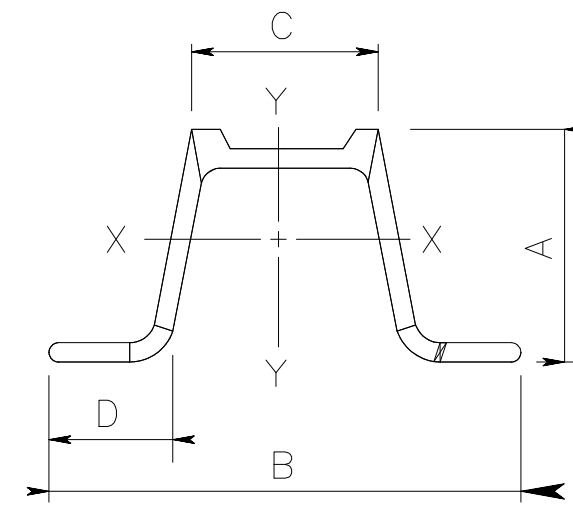
WHEN X IS GREATER THAN 0.75", USE TYPE 1 SPACER BAR
WHEN X IS 0.75" OR LESS, USE TYPE 2 SPACER BAR



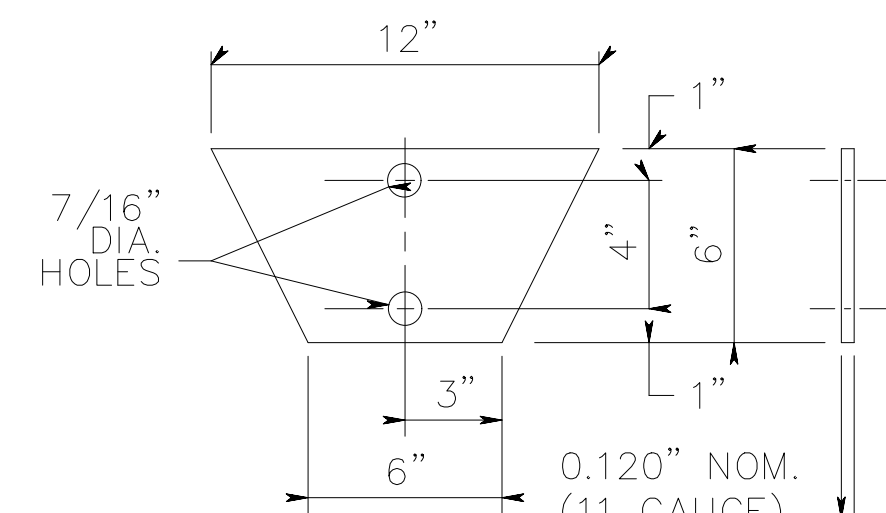
SPACER BAR

WEIGHT* LBS. / FT.	DIMENSIONS (IN)				AREA IN. ²	X AXIS **		Y-Y AXIS	
	"A"	"B"	"C"	"D"		I(IN. 4)	S(IN. 3)	I(IN. 4)	S(IN. 3)
2.50	1.516	3.062	1.278	0.669	0.760	0.228	0.313	0.539	0.352
4.00	1.968	3.500	1.336	0.834	1.187	0.611	0.707	1.161	0.664

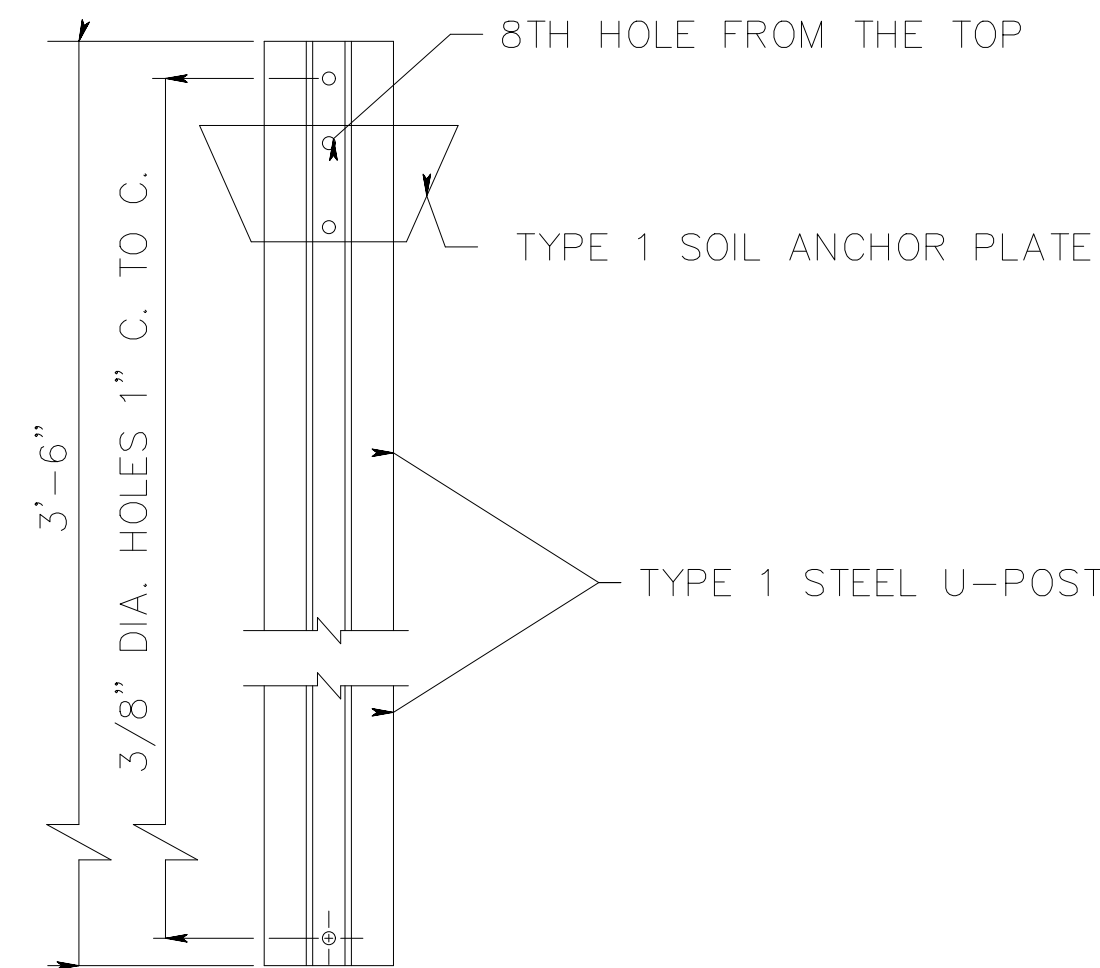
* ' 5%
** GOVERNING SECTION



TYPE 1 STEEL
U-POST



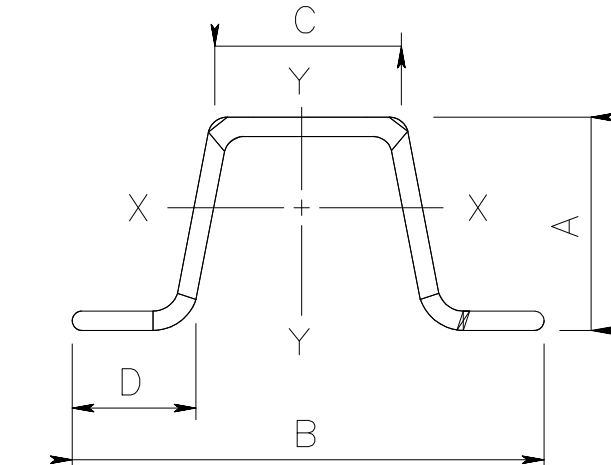
TYPE 1
SOIL ANCHOR PLATE



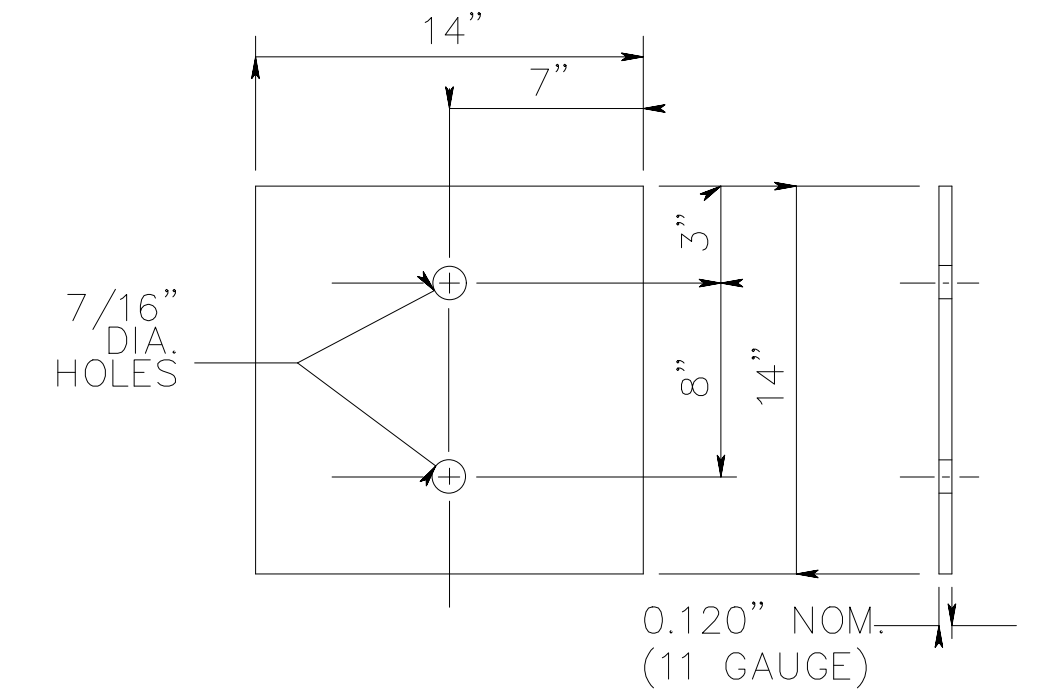
TYPE 1
ANCHOR POST
ASSEMBLY

WEIGHT* LBS. / FT.	DIMENSIONS (IN)				AREA IN. ²	X AXIS **		Y-Y AXIS	
	"A"	"B"	"C"	"D"		I(IN. 4)	S(IN. 3)	I(IN. 4)	S(IN. 3)
2.50	1.549	3.125	1.250	0.625	0.748	0.233	0.289	0.551	0.353
4.00	1.845	3.500	1.625	0.718	1.190	0.500	0.560	1.190	0.690

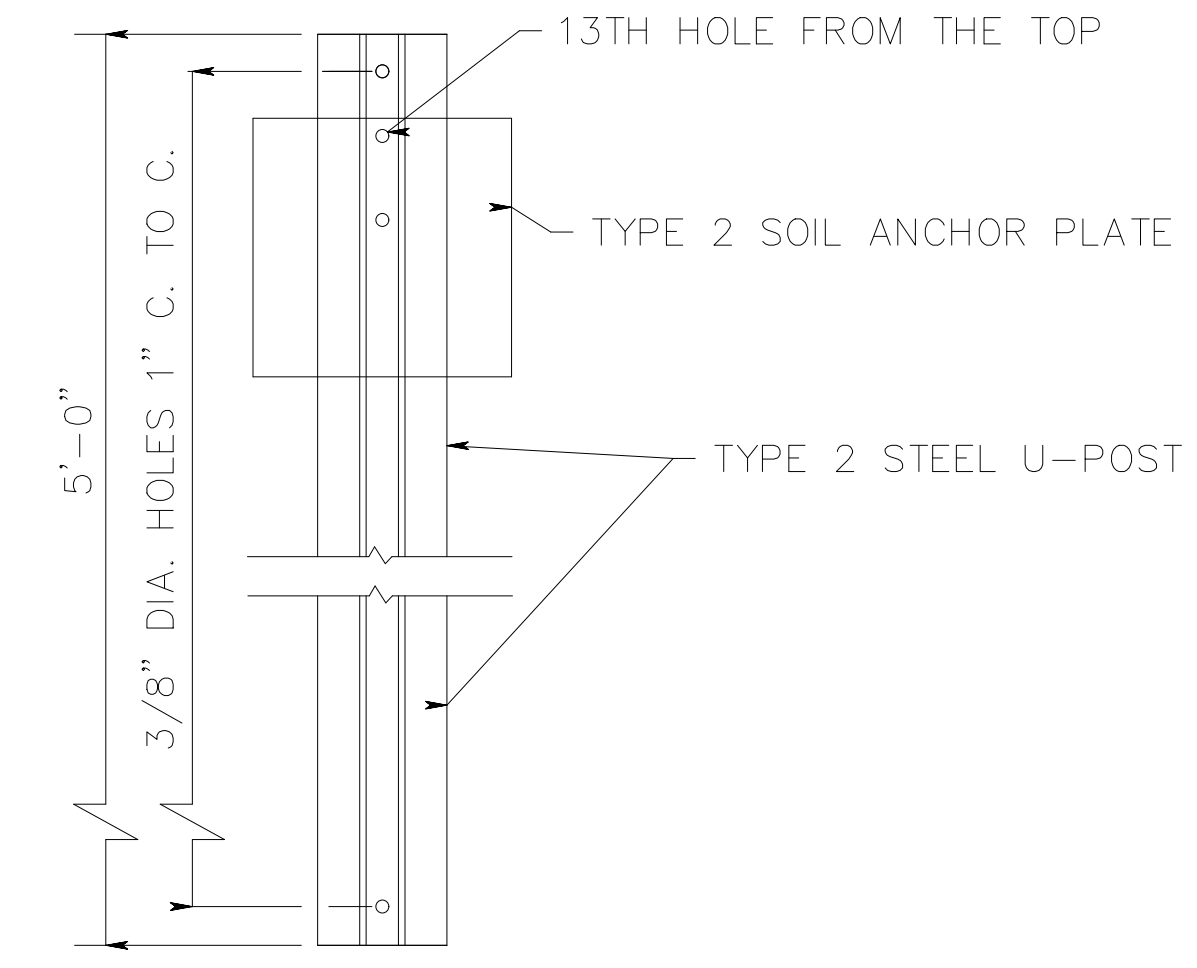
* ' 5%
** GOVERNING SECTION



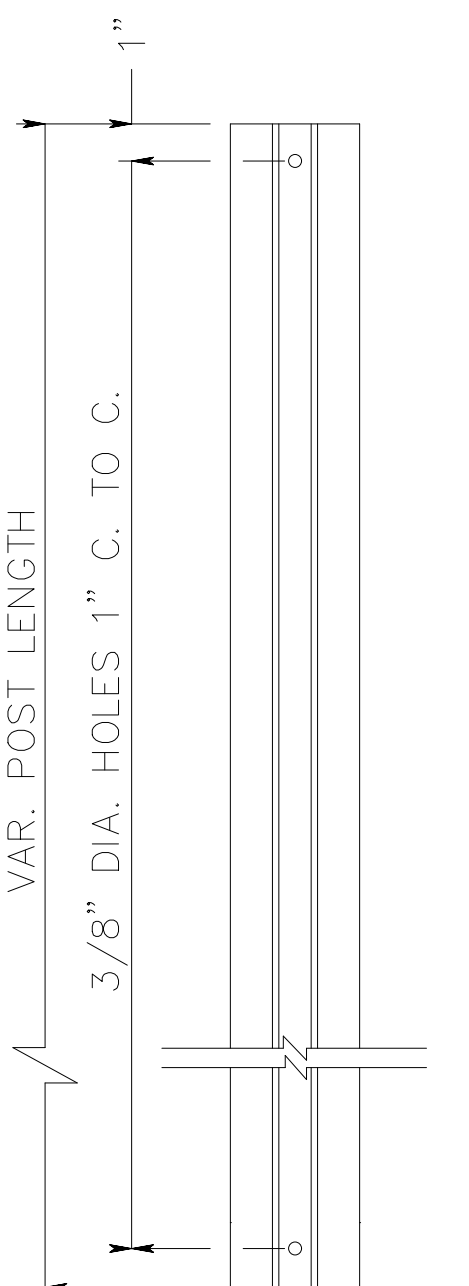
TYPE 2 STEEL
U-POST



TYPE 2
SOIL ANCHOR PLATE



TYPE 2
ANCHOR POST
ASSEMBLY



TOP POST
U-POST

STEEL POSTS, POST CLIPS, SPACING, ETC.
AND TWO PIECE STEEL U-POSTS.

STEEL U-POST SIGN
SUPPORTS

N.T.S.

NOTES:

1. ANCHOR POST AND TOP POST SHALL BE OF EQUAL WEIGHT/FEET.
2. SOIL ANCHOR PLATE SHALL BE ATTACHED TO ALL ANCHOR POSTS.
3. THE MATERIAL FOR THE SOIL ANCHOR PLATES SHALL BE CARBON SHEET STEEL.
4. THE STEEL "U" POST SHALL BE GRADE 60.

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	412 Mount Kemble Avenue Suite G22 Morristown NJ 07960 United States of America Certificate No. 24GA28016600	Client TOWNSHIP OF CHATHAM MORRIS COUNTY, NEW JERSEY	Project Number 507393101-001	B/O 14	Total 19	Designed RS 05/23 Eng check SJA 05/23	Drawn RS 05/23 Coordination	Dwg check SJA 05/23 Approved JKR 05/23	Scale at ARCH D N.T.S.	Status Draft	Rev Rev0	Security STD	Title CHATHAM TOWNSHIP PEDESTRIAN SAFETY AND ADA IMPROVEMENTS CONSTRUCTION DETAILS
	T +1 (908) 730 6000 F +1 (973) 267 2890 www.mottmacamericas.com	JOHN K. RUSCHKE PROFESSIONAL ENGINEER LIC. NO. 24GE0037148 Date 08-03-2023	Drawing Number D-004										

R1-1
24" X 24"
8" C Letters
White message and
border on Red
background.



STOP VIEW

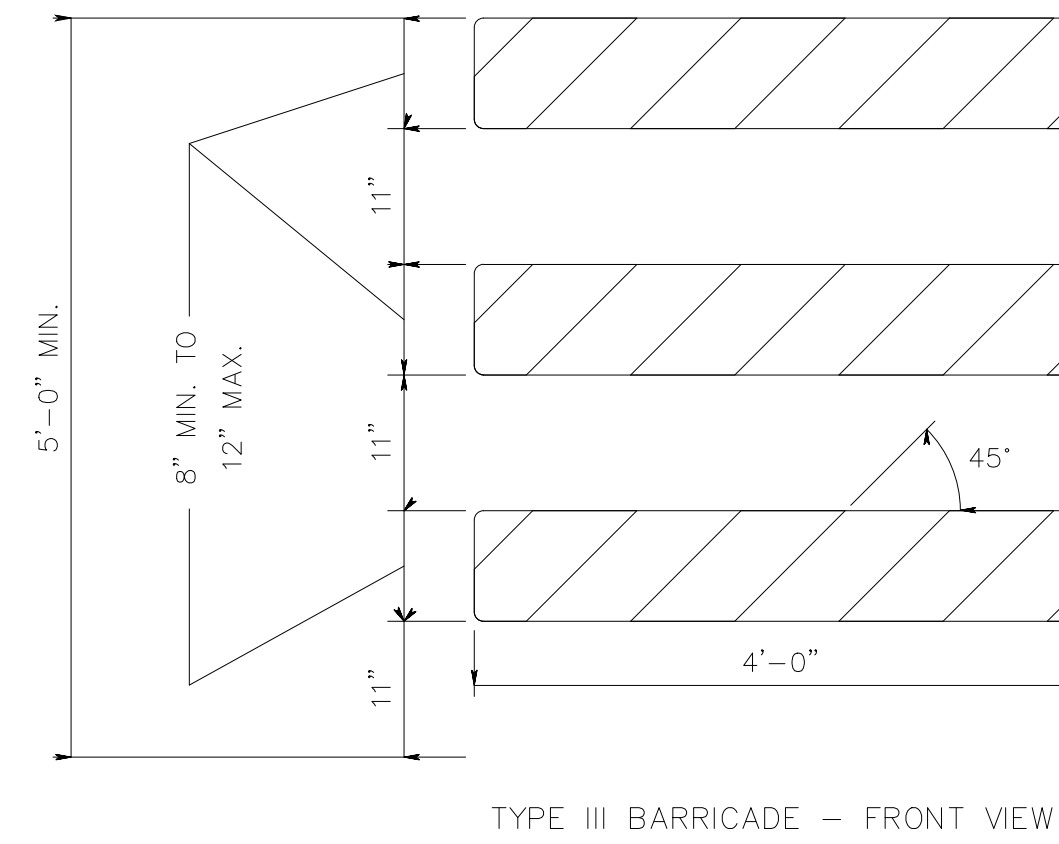
Non-reflective Black
24" X 24"
8" B Letters
Black message and
border on Orange
background.



SLOW VIEW

NOTE: Sign faces shall be reflective sheeting, Type II.

STOP / SLOW PADDLE
NOT TO SCALE



TYPE III BARRICADE - FRONT VIEW

NOTES:

1. ENSURE THE 8" MIN. x 48", TO 12" MAX. x 48" BARRICADE RAILS TO BE ATTACHED ACCORDING TO THE MANUFACTURER'S RECOMMENDATION.
2. ENSURE ORANGE AND SILVER (WHITE) STRIPES TO BE RETROREFLECTIVE SHEETING, ASTM D4956 TYPE III. ALTERNATE ORANGE AND SILVER (WHITE) STRIPES 6" WIDE SLOPING DOWNWARD AT AN ANGLE OF 45 DEGREES IN THE DIRECTION TRAFFIC IS TO PASS.
3. THE FRAMING, RAILS, AND BALLAST FOR BREAKAWAY BARRICADE TO BE NCHRP-350 CRASHED TESTED AND FHWA APPROVED.
4. IF NECESSARY, FABRICATE THE BALLAST AND PLACE ACCORDING TO THE MANUFACTURER'S RECOMMENDATION.

CD-159-1.3

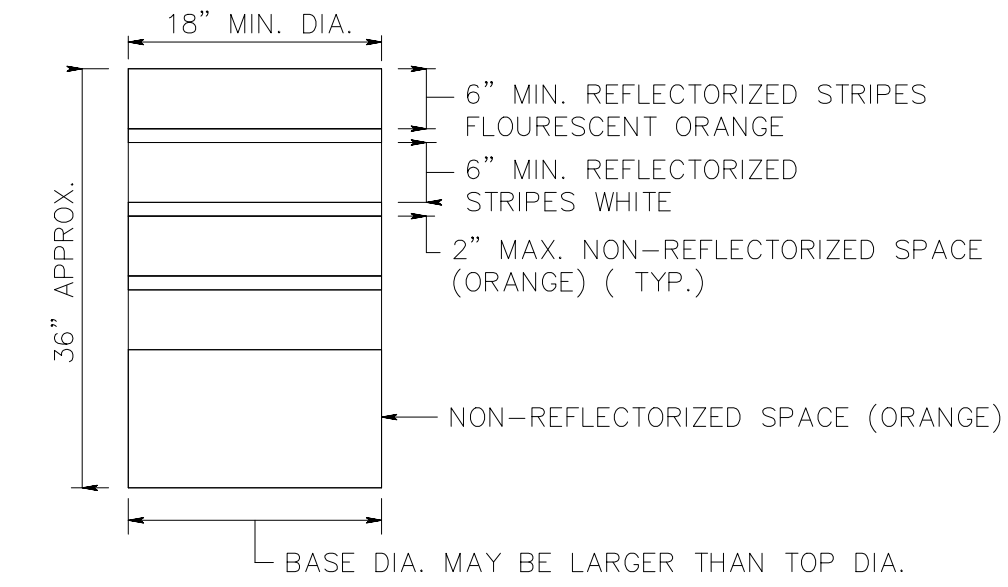
BREAKAWAY BARRICADES
NOT TO SCALE

ENSURE DRUMS ARE MADE OF ORANGE PLASTIC WITH A MINIMUM OF FOUR ALTERNATE FLUORESCENT ORANGE AND WHITE RETROREFLECTIVE STRIPES, IF THERE ARE

ENSURE RETROREFLECTIVE SHEETING FOR STRIPES CONFORMS WITH ASTM D4956 TYPE VII OR VIII WITH S2 REQUIREMENTS.

ENSURE THE TOP OF THE DRUM IS NOT OPEN. CONSTRUCT DRUMS TO INHIBIT ROLLING IF KNOCKED OVER.

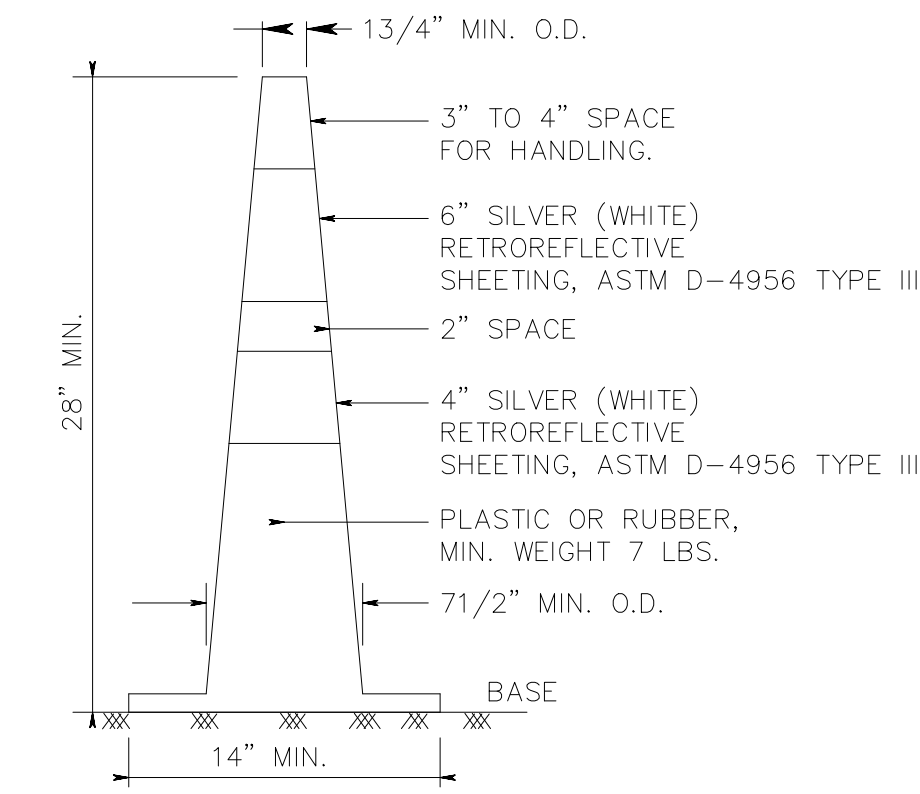
ENSURE THE REFLECTORIZED AREA OF DRUMS IS ROUND EXCEPT OTHER SHAPES, WHICH PROVIDE THE SAME VISIBILITY AS AN 18 INCH DIAMETER ROUND DRUM REGARDLESS OF ORIENTATION, MAY BE USED.



WHEN BALLAST IS REQUIRED BY THE RE, USE SAND. THE MAXIMUM WEIGHT OF THE BALLAST IS 50 LBS. AND IS TO BE LOCATED APPROXIMATELY AT GROUND LEVEL. ALTERNATE TYPES OF BALLAST MUST BE APPROVED BY THE RE.

CD-159-1.1

DRUMS
NOT TO SCALE



NOTES:

TRAFFIC CONES MUST BE PREDOMINATELY ORANGE IN COLOR.
BASES MAY BE OF BREAKAWAY BALLASTED TYPE.
MINOR MANUFACTURER'S VARIATIONS MAY BE ACCEPTABLE UPON APPROVAL OF THE RE.

CD-159-1.2

TRAFFIC CONES
NOT TO SCALE

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Client
**TOWNSHIP OF CHATHAM
MORRIS COUNTY, NEW JERSEY**

JOHN K. RUSCHKE
PROFESSIONAL ENGINEER LIC. NO. 24GE0037148

John Ruschke
Date 08-03-2023

Rev	Date	Drawn	Description	Ch'k'd	App'd

Project Number	507393101-001	B/O	15	Total	19
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Designed	RS	05/23	Eng check	SJA	05/23
Drawn	RS	05/23	Coordination		
Dwg check	SJA	05/23	Approved	JKR	05/23
Scale at ARCH D	Status	Rev	Security		
N.T.S.	Draft	Rev0	STD		
Drawing Number T-001					

Title
**CHATHAM TOWNSHIP
PEDESTRIAN SAFETY AND ADA
IMPROVEMENTS**

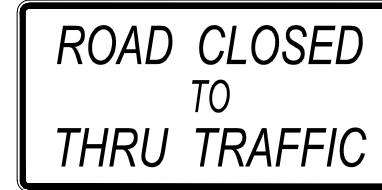
TRAFFIC CONTROL DETAILS



G20-1
60" X 36"



M4-11
30" X 24"
M4-11 (S)
48" X 36"



R11-4
60" X 30"



W8-10
48" X 48"



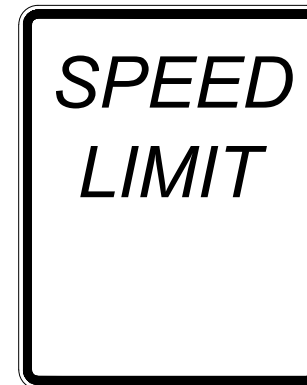
W20-5 (L, R or C)
48" X 48"



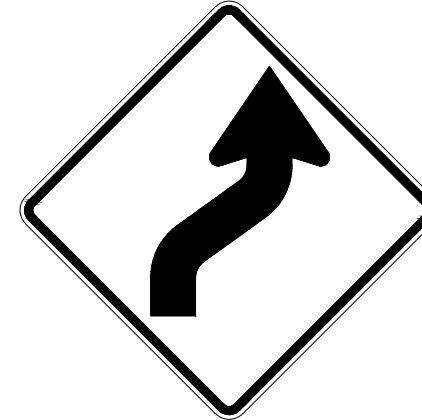
BLACK ON ORANGE
48" X 48"



G20-2
60" X 24"



R2-1
36" X 48"
R2-1 (S)
48" X 60"



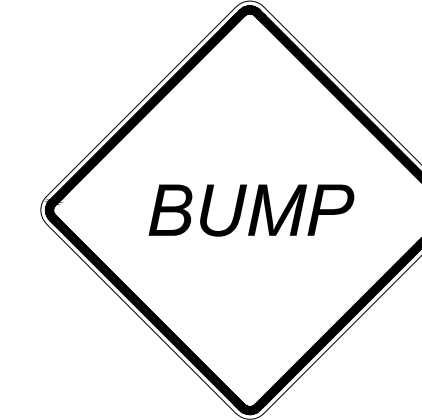
W1-4 (S) (L&R)
48" X 48"



W13-1
18" X 18"
W13-1 (S)
24" X 24"



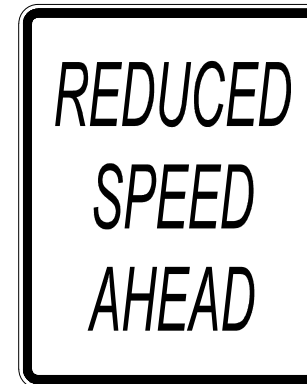
W20-7a (S)
48" X 48"



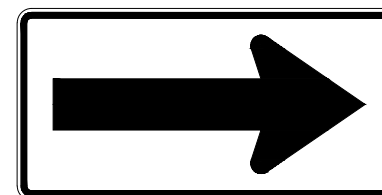
W8-1
48" X 48"



M4-9 (L&R)
30" X 24"
M4-9 (S) (L&R)
48" X 36"



R2-5a
36" X 48"
R2-5a (S)
48" X 60"



W1-6 (L&R)
48" X 24"
W1-6 (S) (L&R)
60" X 30"



W20-1
48" X 48"



W21-5 (S)
48" X 48"



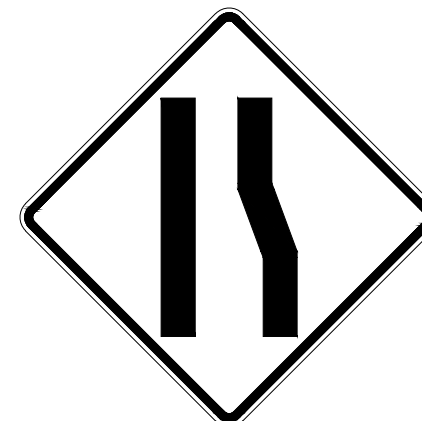
W8-9a
48" X 48"



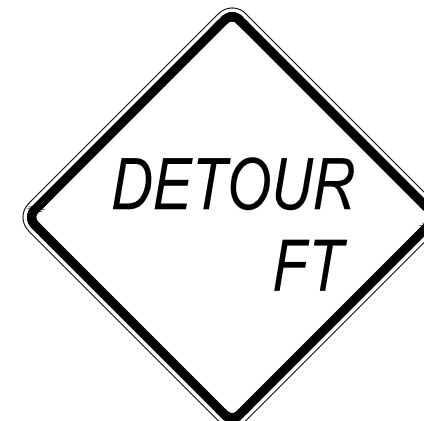
M4-9X (L&R)
30" X 24"
M4-9X (S) (L&R)
48" X 36"



R4-1
24" X 30"



W4-2 (S) (L&R)
48" X 48"



W20-2
48" X 48"



BLACK ON ORANGE
48" X 48"



W8-14
48" X 48"



M4-9X
30" X 24"
M4-9X (S)
48" X 36"



R11-2
48" X 30"



W5-1 (S)
48" X 48"



W20-3
48" X 48"



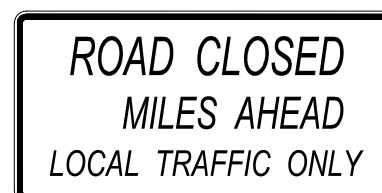
24" X 24"
30" X 30" (S)



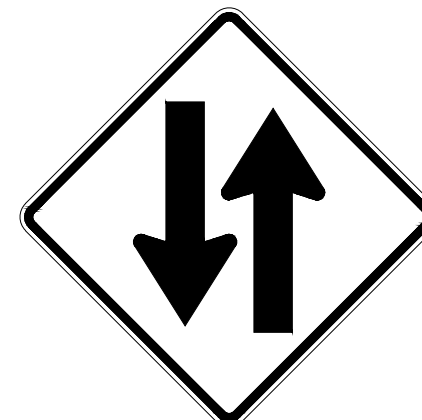
W8-14a
48" X 48"



M4-10 (L&R)
48" X 18"



R11-3
60" X 30"



W6-3
48" X 48"



W20-4
48" X 48"



24" X 24"
30" X 30" (S)



W99-2
48" X 48"

GENERAL NOTES

1. INSTALLATION, DIMENSIONS, COLORS AND DETAILS OF VARIOUS SIZE SIGNS AND ACCESSORY PANELS TO FOLLOW STANDARDS IN THE CURRENT "STANDARD HIGHWAY SIGN PUBLICATION" AND THE CURRENT "MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES FOR STREETS AND HIGHWAYS."
2. (S) REPRESENTS A SPECIAL SIZE SIGN.
3. LETTERS AND NUMBERS SHALL CONFORM TO THE CURRENT MANUAL "STANDARD ALPHABETS FOR HIGHWAY SIGNS" U.S. DEPARTMENT OF TRANSPORTATION, FEDERAL HIGHWAY ADMINISTRATION.
4. THE CONTRACTOR SHALL OBTAIN THE APPROVAL OF THE ENGINEER FOR THE DISTANCE TO BE USED ON THE ADVANCE WARNING SIGNS, AND FOR THE SPEED LIMIT TO BE USED ON THE R2-1 SIGN.
- 5.

DISTANCE LEGEND	
SIGN NUMBER FOLLOWED BY LETTER	DISTANCE
A	1500'
B	1000'
C	500'
D	MILE
E	___ MILES AHEAD
F	AHEAD

BACKING MATERIAL

1. ALUMINUM SHALL BE FLAT SHEET OF 6061-T6 ALLOY, .100" GAUGE.

TEMPORARY SIGN SUPPORTS

1. SIGN SUPPORTS SHALL BE OF WELL SEASONED LUMBER, S4S, FREE OF SPLITS, KNOTS AND WARPS OR OF STEEL OR ALUMINUM COMPONENTS.
2. WOOD POSTS SHALL HAVE A UNIFORM CROSS-SECTION AND SHALL NOT EXCEED THE FOLLOWING DIMENSIONS FOR:
SINGLE POST = 4" X 6"
TWO POSTS = 3" X 6" or 4" X 5"
THREE POST = 3" X 5" or 4" X 4"
3. NO BRACING IS PERMITTED. VERTICAL CLEARANCES FOR SIGNS MOUNTED ON WOOD SUPPORTS SHALL BE 7' DESIRABLE, 6' MINIMUM. EMBEDMENT DEPTH FOR THE WOOD POSTS SHALL NOT EXCEED 3.5'.
4. STEEL OR ALUMINUM POSTS SHALL BE IN ACCORDANCE WITH THE STANDARD DETAIL FOR "SELECTIVE DIRECTIONAL SIGNS, CONSTRUCTION AND "M", "W" AND "R" BENDAWAY SIGN SUPPORTS".
5. TEMPORARY SIGN SUPPORTS NOT MEETING THIS CRITERIA SHALL BE SHIELDED BY A LONGITUDINAL BARRIER OR CRASH CUSHION.

SIGN FACES

1. SIGN FACES SHALL BE REFLECTIVE SHEETING, TYPE 11.

FASTENING

1. ALL SIGNS SHALL BE SECURELY FASTENED TO THEIR SUPPORTS WITH BOLTS, NUTS AND WASHERS OF ALUMINUM (2024-T4 ALLOY) OR HOT-DIP GALVANIZED STEEL (A.S.T.M. 153).

NOTE: REGARDING SIGN W99-2, THE BORDER, THE WORDS "GIVE US A", "SLOW DOWN" AND THE BRAKE PEDAL ARE BLACK, LEAVING THE WORD "BRAKE" ORANGE. AT PRESENT SIGN LAYOUT AND SYMBOL UNAVAILABLE.

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Client

TOWNSHIP OF CHATHAM
MORRIS COUNTY, NEW JERSEY

JOHN K. RUSCHKE
PROFESSIONAL ENGINEER LIC. NO. 24GE0037148

John Ruschke
Date 08-03-2023

Project Number
507393101-001

B/O
16

Total
19

Designed	RS	05/23	Eng check	SJA	05/23
Drawn	RS	05/23	Coordination		
Dwg check	SJA	05/23	Approved	JKR	05/23
Scale at ARCH D	Status	Rev	Security		
N.T.S.	Draft	Rev0	STD		

Drawing Number
T-002

CHATHAM TOWNSHIP
PEDESTRIAN SAFETY AND ADA
IMPROVEMENTS

TRAFFIC DETAILS

Rev	Date	Drawn	Description	Ch'k'd	App'd

TRAFFIC NOTES:

1. ADVANCE WARNING SIGNS DISTANCES, AND TAPER LENGTHS MAY BE EXTENDED, AT DIRECTION OF THE DEPARTMENT, TO ADJUST FOR REDUCED VISIBILITY DUE TO HORIZONTAL AND VERTICAL CURVATURE OF THE ROADWAY.
2. THE APPROXIMATE LOCATIONS OF THE ILLUMINATED FLASHING ARROW BOARDS ARE SHOWN ON THE TRAFFIC CONTROL PLANS. THESE LOCATIONS MAY BE MODIFIED AS APPROVED BY RE TO ADJUST FOR VISIBILITY DUE TO HORIZONTAL OR VERTICAL CURVATURE OF THE ROADWAY OR TO POSITION AT A SAFER LOCATION. ILLUMINATED FLASHING ARROW BOARDS ARE TO BE USED FOR TEMPORARY LANE CLOSINGS AND AT LOCATIONS SHOWN ON THE TRAFFIC CONTROL PLANS.
3. PRIOR TO ANY ROAD CONSTRUCTION, TRAFFIC CONTROL SIGNS AND DEVICES SHALL BE IN PLACE.
4. RAMPS AND/OR SIDE STREETS ENTERING THE ROADWAY AFTER THE FIRST ADVANCE WARNING SIGN SHALL BE PROVIDED WITH AT LEAST ONE W20-IF SIGN (ROAD WORK AHEAD) AS A MINIMUM.
5. ALL EXISTING ROAD SIGNS, PAVEMENT MARKINGS AND/OR PLOWABLE PAVEMENT REFLECTORS WHICH CONFLICT WITH THE PROPOSED TRAFFIC CONTROL PLAN SHALL BE COVERED, REMOVED OR RELOCATED AS DIRECTED BY THE RE.
6. CONFLICTING OR NON-OPERATING SIGNAL INDICATIONS ON EITHER THE EXISTING, TEMPORARY,OR PROPOSED TRAFFIC SIGNAL SYSTEMS SHALL BE BAGGED OR COVERED.
7. MAINTENANCE AND PROTECTION OF TRAFFIC SHALL BE IN ACCORDANCE WITH THE MANUAL OF UNIFORM TRAFFIC CONTROL DEVICES – PART VI "STANDARDS AND GUIDES FOR TRAFFIC CONTROL FOR STREET AND HIGHWAY CONSTRUCTION, MAINTENANCE, UTILITY, AND INCIDENT MANAGEMENT OPERATIONS", UNLESS OTHERWISE NOTED IN THE PLANS AND SPECIFICATIONS.
8. CONSTRUCTION SIGN W99-2 (GIVE US A BRAKE) SHALL BE LOCATED 200 FEET IN ADVANCE OF PROJECT LIMITS.
9. A W1-6 (ARROW) SIGN MOUNTED ON A BREAKAWAY BARRICADE AND CENTERED ON THE CLOSED WIDTH SHALL BE LOCATED 100 FEET BEYOND EACH INTERSECTION OR MAIN ACCESS POINT WITHIN THE AREA OF A LANE OR SHOULDER CLOSURE.
10. CONSTRUCTION SIGNS R11-4 (ROAD CLOSED TO THRU TRAFFIC) SHALL BE PLACED AT THE INTERSECTING STREETS WHICH ARE CLOSED TO TRAFFIC BECAUSE OF CONSTRUCTION.
11. CONSTRUCTION SIGNS W8-9A (SYMBOL FOR UNEVEN PAVEMENT) AND W8-14A (GROOVED PAVEMENT) SHALL BE USED WHEN SUCH PAVEMENT CONDITIONS EXIST. THE PLACEMENT OF THESE SIGNS SHALL BE AS DIRECTED BY THE RE.
12. MOVING WORK AREAS IN A LANE CLOSURE REQUIRE A TRAILER MOUNTED ILLUMINATED FLASHING ARROW TO REMAIN AT THE END OF THE TAPER, THE TRAFFIC CONTROL TRUCK WITH MOUNTED CRASH CUSHION THAT SHALL MOVE WITH THE WORK AREAS TO KEEP A 70 FEET MIN. AND 150 FEET MAX. BUFFER IN ADVANCE OF EACH WORK AREA.
13. THE CONTRACTOR SHALL SUBMIT A PLAN FOR THE SAFE ACCESS OF CONSTRUCTION VEHICLES THROUGHOUT THE WORK SITE WHERE SPACE CONSTRAINTS PREVENT THE USE OF LANE CLOSURES. THE PLAN SHALL BE SUBMITTED TO THE RE IN ACCORDANCE WITH THE STANDARD SPECIFICATIONS.
14. ALL EXCAVATED AREAS WITHIN OR ADJACENT TO THE ROADWAY SHALL BE BACKFILLED AND PLACED ON AT LEAST 6H : 1V SLOPE BEFORE THE END OF EACH WORK DAY. OTHER EXCAVATED AREA WITHIN THE CLEAR ZONE SHALL BE BACKFILLED.
15. WHERE REQUIRED, THE CONTRACTOR SHALL MAKE PROVISIONS FOR MAINTAINING PEDESTRIAN CROSSING LOCATIONS AND TYPE AS DIRECTED BY THE RE.
16. BITUMINOUS CONCRETE PLACED DURING THE VARIOUS CONSTRUCTION STAGES SHALL BE TRANSITIONED ON A MINIMUM 20H : 1V SLOPE TO MEET THE ADJACENT EXISTING GRADE AT THE LONGITUDINAL AND TRANSVERSE LIMITS OF THE STAGE CONSTRUCTION AREAS UNLESS OTHERWISE NOTED ON THE STAGE CONSTRUCTION PLANS.
17. THE PLACEMENT AND OR RELOCATION OF PRECAST CONCRETE CURB, CONSTRUCTION BARRIER SHALL BE DONE DURING APPROVED OFF-PEAK HOURS WHEN TRAFFIC MAY BE REDUCED TO ONE LANE IN EACH DIRECTION.
18. CONSTRUCTION ZONE SPEED LIMIT WILL BE DETERMINED BY THE TRAFFIC SIGNAL & SAFETY ENGINEERING, REGIONAL TRAFFIC ENGINEER – WORK ZONE, AT THE TIME OF OR DURING CONSTRUCTION, AS REQUESTED BY THE R.E..
19. THE SPEED LIMIT, R2-1 (BLACK ON WHITE) WITH ADDED WORK ZONE PLATE (BLACK ON ORANGE) SIGNS SHALL BE LOCATED THROUGH WORK AREAS AS DIRECTED BY THE TRAFFIC SIGNAL & SAFETY ENGINEERING REGIONAL TRAFFIC ENGINEER – WORK ZONE.
20. THE REDUCED SPEED AHEAD SIGN, W3-5(S) (BLACK ON ORANGE) SHALL BE LOCATED IN ADVANCE OF SPEED LIMIT R2-1 SIGNS WHICH REDUCE THE NORMAL POSTED SPEED LIMIT THROUGH THE CONSTRUCTION ZONE.
21. TRAFFIC FINES DOUBLED IN WORK AREA R(NJ)5-17(S) , 4 FEET BY 2.5 FEET SIGN SHALL BE LOCATED 500 FEET AFTER THE FIRST ADVANCE WARNING SIGN, (W20 SERIES) AT EACH WORK AREA LOCATED WITHIN URBAN AREAS. THIS SIGN SHALL ALSO BE USED ON PROJECTS REQUIRING MOVING OPERATIONS IN WHICH CASE THE SIGN SHALL BE MOUNTED ON A SLOW MOVING CONSTRUCTION VEHICLE.
22. THE FINAL HMA SURFACE PAVEMENT SHALL NOT BE CONSTRUCTED UNTIL THE FINAL STAGE OF THE PROJECT UNLESS OTHERWISE DIRECTED BY THE RE OR INDICATED ON THE PLANS. MANHOLES AND INLETS SHALL BE SET TO FINISHED GRADE AND TEMPORARY PAVEMENT RAMPS ARE TO BE CONSTRUCTED AROUND THEM WITH A MINIMUM 20H : 1V SLOPE IN ALL DIRECTIONS USING HOT MIX ASPHALT PAVEMENT. THIS TEMPORARY MATERIAL WILL BE REMOVED IMMEDIATELY PRIOR TO PLACING THE SURFACE COURSE.

23. TRAFFIC CONTROL DEVICES FOR LANE CLOSURES INCLUDING SIGNS, CONES, BARRICADES, ETC. SHALL BE PLACED AS SHOWN ON PLANS. SIGNS SHALL NOT BE PLACED WITHOUT ACTUAL LANE CLOSURES AND SHALL BE IMMEDIATELY REMOVED UPON REMOVAL OF THE CLOSURES.
24. CONES MAY BE SUBSTITUTED FOR DRUMS AND INSTALLED UPON THE APPROVAL OF THE RE.
25. TRAFFIC IMPACT NOTICES AND CHANGES

- A. TERMS:
WHEN THE FOLLOWING TERMS ARE USED, THE INTENT AND MEANING SHALL BE AS FOLLOWS:
- i. IMPACTS TO NORMAL TRAFFIC FLOW – WORK THAT REQUIRES A PORTION OF THE PAVED ROADWAY BEING BLOCKED OR CLOSED WITH SAFETY DEVICES OR VEHICLES, INCLUDING, BUT NOT LIMITED TO, FULL OR PARTIAL LANE CLOSURES, FULL OR PARTIAL RAMP CLOSURES, SHOULDER CLOSURES, MOVING OPERATIONS SUCH AS TRAFFIC STRIPING OR SWEEPING, LANE SHIFTS, OR ALTERNATING TRAFFIC. THIS APPLIES EVEN WHEN DETOURS ARE PROVIDED.
 - ii. TEMPORARY LANE CLOSURES – WORK DESCRIBED UNDER "IMPACTS TO NORMAL TRAFFIC FLOW" WHICH IS ROUTINELY SET UP AND REMOVED ON A DAILY BASIS.
 - iii. PERMANENT LANE CLOSURES – WORK DESCRIBED UNDER "IMPACTS TO NORMAL TRAFFIC FLOW" WHICH REMAINS IN PLACE CONTINUOUSLY FOR 24 HOURS OR MORE.

B. ADVANCE NOTICES
FOR THE INITIAL START OF WORK THAT REQUIRES "IMPACTS TO NORMAL TRAFFIC FLOW", THE CONTRACTOR SHALL NOTIFY THE RE IN WRITING, ON THE ADVANCE FORM TO-103 PROVIDED BY THE DEPARTMENT, OF THE PROPOSED DATE. THE NOTICE SHALL BE SUBMITTED AT LEAST TWENTY-EIGHT CALENDAR DAYS, BUT NOT MORE THAN SIXTY CALENDAR DAYS, BEFORE THE PROPOSED DATE. START OF WORK THAT IMPACTS NORMAL TRAFFIC FLOW WILL NOT BE PERMITTED PRIOR TO THE DATE STATED IN THE NOTICE. THE CONTRACTOR SHALL CONFIRM, IN WRITING TO THE RE, THE PROPOSED DATE SEVEN (AND/OR FOURTEEN) CALENDAR DAYS BEFORE STARTING THE ESTABLISHMENT OF THE TRAFFIC CONTROL MEASURES FOR THE TRAFFIC IMPACT. THE CONTRACTOR SHALL IMMEDIATELY NOTIFY THE RE IF THE PROPOSED ESTABLISHMENT CAN NOT BE COMPLETED ON THE PROPOSED DATE.

FOR A "PERMANENT LANE CLOSURE", THE CONTRACTOR SHALL NOTIFY THE RE IN WRITING, ON ADVANCE FORM TO-103, OF THE PROPOSED DATE A NEW TRAFFIC PATTERN WILL BE ESTABLISHED. THE NOTICE SHALL BE SUBMITTED AT LEAST TWENTY-EIGHT CALENDAR DAYS, BUT NOT MORE THAN SIXTY CALENDAR DAYS, IN ADVANCE OF THE PROPOSED DATE. START OF A NEW TRAFFIC PATTERN WILL NOT BE PERMITTED PRIOR TO THE DATE STATED IN THE NOTICE. THE CONTRACTOR SHALL CONFIRM, IN WRITING TO THE RE, THE PROPOSED DATE OF THE NEW TRAFFIC PATTERN SEVEN (AND/OR FOURTEEN) DAYS BEFORE STARTING TRAFFIC CONTROL MEASURES FOR THE ESTABLISHMENT OF THE NEW PATTERN. THE CONTRACTOR SHALL IMMEDIATELY NOTIFY THE RE IF THE PROPOSED ESTABLISHMENT CAN NOT BE COMPLETED ON THE PROPOSED DATE.

STARTING THE ESTABLISHMENT OF A NEW PERMANENT TRAFFIC PATTERN SHALL BEGIN NO EARLIER THAN 11:00 PM FRIDAY AND SHALL BE COMPLETED AND READY FOR OPERATIONS BY 6:00 PM THE FOLLOWING SUNDAY. THE ESTABLISHMENT SHALL BE COMPLETED IN ACCORDANCE WITH THE LANE CLOSURE HOURS SPECIFIED IN THE CONTRACT.

ADVANCE NOTICES SENT PRIOR TO THE PRE-CONSTRUCTION MEETING SHALL BE ADDRESSED TO THE CONTACT PERSON AS SPECIFIED IN SUBSECTION 101.04 OF THE SPECIAL PROVISIONS.

C. PROGRESS NOTICES
ALL "IMPACTS TO NORMAL TRAFFIC FLOW" SCHEDULED FOR THE SEVEN DAY PERIOD STARTING ON THE FOLLOWING MONDAY SHALL BE SUBMITTED TO THE RE BY 9:00 AM OF EACH FRIDAY ON WEEKLY FORM TO-100 PROVIDED BY THE DEPARTMENT.

EACH DAY OF "TEMPORARY LANE CLOSURES" SHALL BE SUBMITTED TO THE RE BY 9:00 AM THE DAY IN ADVANCE OF THE START OF THOSE OPERATIONS ON DAILY FORM TO-101 PROVIDED BY THE DEPARTMENT.



"TEMPORARY LANE CLOSURES" FOR WEEKENDS SHALL BE SUBMITTED TO THE RE BY 9:00 AM ON THE IMMEDIATELY PRECEDING FRIDAY ON THE DAILY FORM TO-101 PROVIDED BY THE DEPARTMENT.

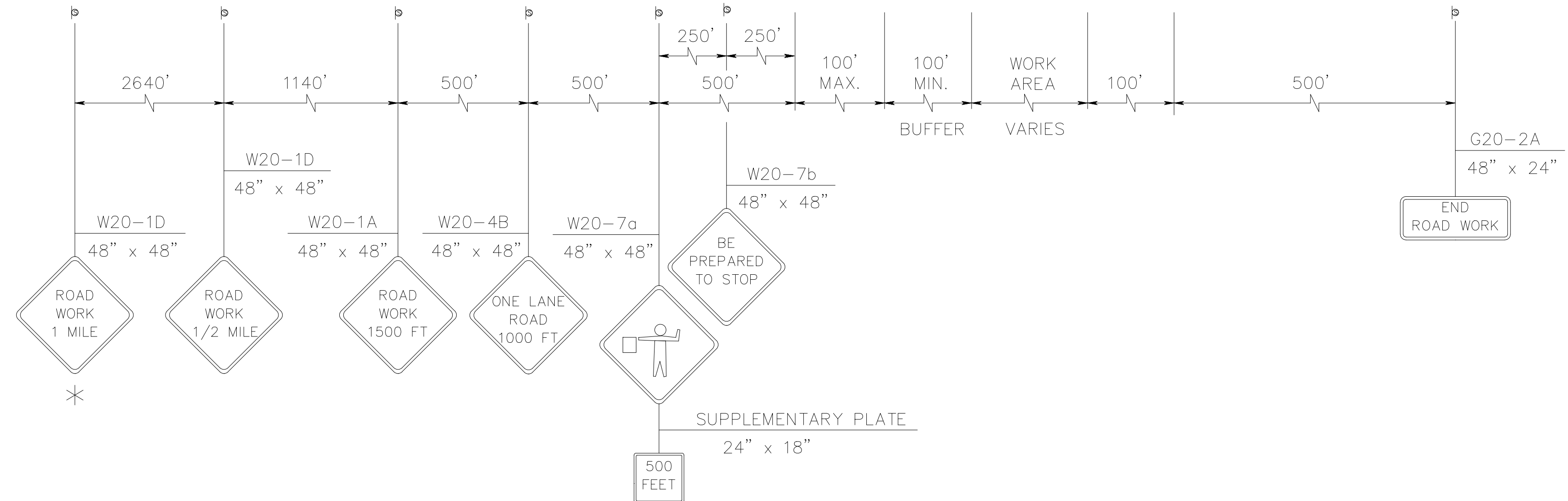
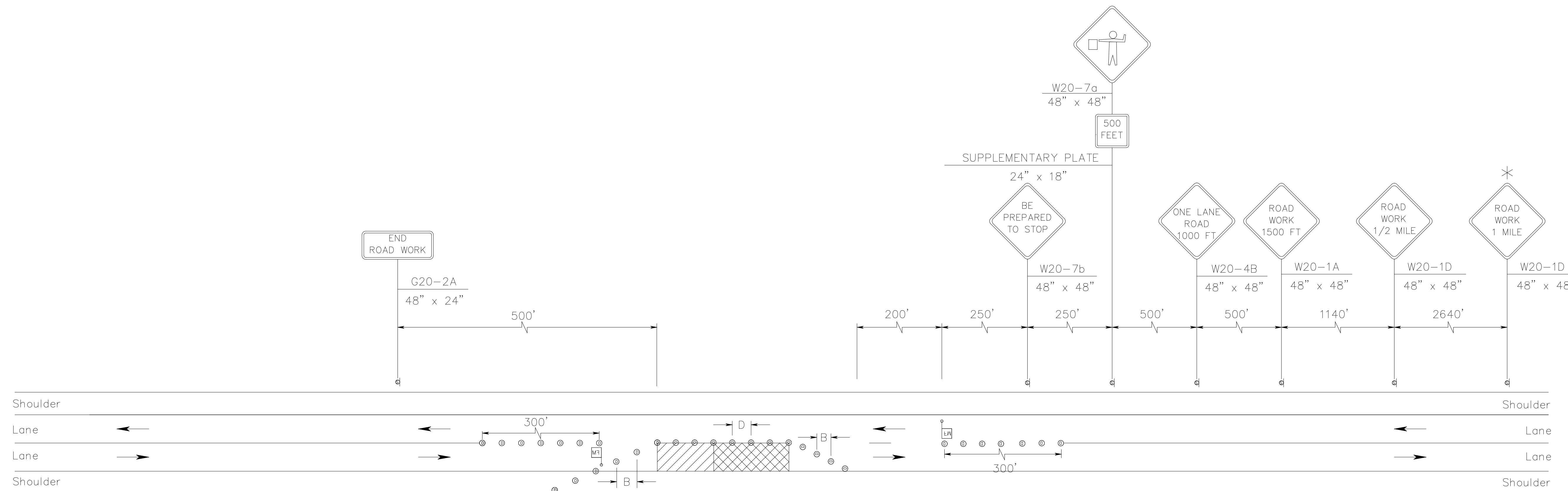
D. CHANGES TO THE SCHEDULED CLOSURES
REQUEST FOR A CHANGE TO THE TRAFFIC CONTROL REQUIREMENTS IN THE CONTRACT DOCUMENTS SHALL BE SUBMITTED IN WRITING TO THE RE AS FOLLOWS:

CHANGES TO THE SCHEDULED HOURS FOR "TEMPORARY LANE CLOSURES" SHALL BE SUBMITTED TO THE R.E. AT LEAST EIGHT CALENDAR DAYS IN ADVANCE OF WHEN THE CHANGE IS PROPOSED TO START.

OTHER PROPOSED CHANGES TO "TEMPORARY LANE CLOSURES" AND ALL CHANGES TO "PERMANENT LANE CLOSURES" SHALL BE SUBMITTED TO THE RE AS SPECIFIED IN THE SPECIFICATIONS.

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	412 Mount Kemble Avenue Suite G22 Morristown NJ 07960 United States of America Certificate No. 24GA28016600 T +1 (908) 730 6000 F +1 (973) 267 2890 www.mottmacamericas.com	Client TOWNSHIP OF CHATHAM MORRIS COUNTY, NEW JERSEY	JOHN K. RUSCHKE PROFESSIONAL ENGINEER LIC. NO. 24GE0037148  Date 08-03-2023	Designed	RS	05/23	Eng check	SJA	05/23	Title CHATHAM TOWNSHIP PEDESTRIAN SAFETY AND ADA IMPROVEMENTS TRAFFIC CONTROL DETAILS
				Drawn	RS	05/23	Coordination			
				Dwg check	SJA	05/23	Approved	JKR	05/23	
				Scale at ARCH D	Status	Rev	Security			
				N.T.S.	Draft	Rev0	STD			
				Project Number	B/O	Total	Drawing Number			
				507393101-001	17	19	T-003			
				Rev	Date	Drawn	Description	Ch'k'd	App'd	



* THIS SIGN SHALL BE INSTALLED FOR ROADS WITH A SPEED LIMIT OF 45 M.P.H. OR GREATER UNLESS OTHERWISE DIRECTED BY THE RE.

NOTE:
SEE RECOMMENDED TAPER LENGTH AND SPACING TABLE BELOW.

2 LANES, UNDIVIDED, LANE & SHOULDER CLOSING W/FLAGGING

REGULATORY APPROACH SPEED OF TRAFFIC MILES / HOUR	MINIMUM TAPER RATIO IN LENGTH PER FOOT OF WIDTH			MAXIMUM DEVICE (B) SPACING ALONG TANGENTS IN FEET	MAXIMUM DEVICE (D) SPACING ALONG TANGENTS IN FEET
	10'	11'	12'		
25	10.5:1	105	115	25	50
30	15:1	150	165	30	60
35	20.5:1	205	225	35	70
40	27:1	270	300	40	80
45	45:1	450	495	45	90
50	50:1	500	550	50	100
55	55:1	550	605	55	110
60	60:1	600	660	60	120
65	65:1	650	715	65	130

NOTE:
THE MAXIMUM DEVICE SPACING ALONG CURVES IS DEFINED FOR TAPERS (B) IN THE ABOVE TABLE.

RECOMMENDED TAPER LENGTH AND SPACING

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Rev	Date	Drawn	Description	Ch'k'd	App'd

JOHN K. RUSCHKE
PROFESSIONAL ENGINEER LIC. NO. 24GE0037148

John Ruschke
Date 08-03-2023

Project Number: **507393101-001**

B/O: **18** Total: **19**

Designed	RS	05/23	Eng check	SJA	05/23
Drawn	RS	05/23	Coordination		
Dwg check	SJA	05/23	Approved	JKR	05/23
Scale at ARCH D	Status	Rev	Security		
N.T.S.	Draft	Rev0	STD		
Drawing Number			T-004		

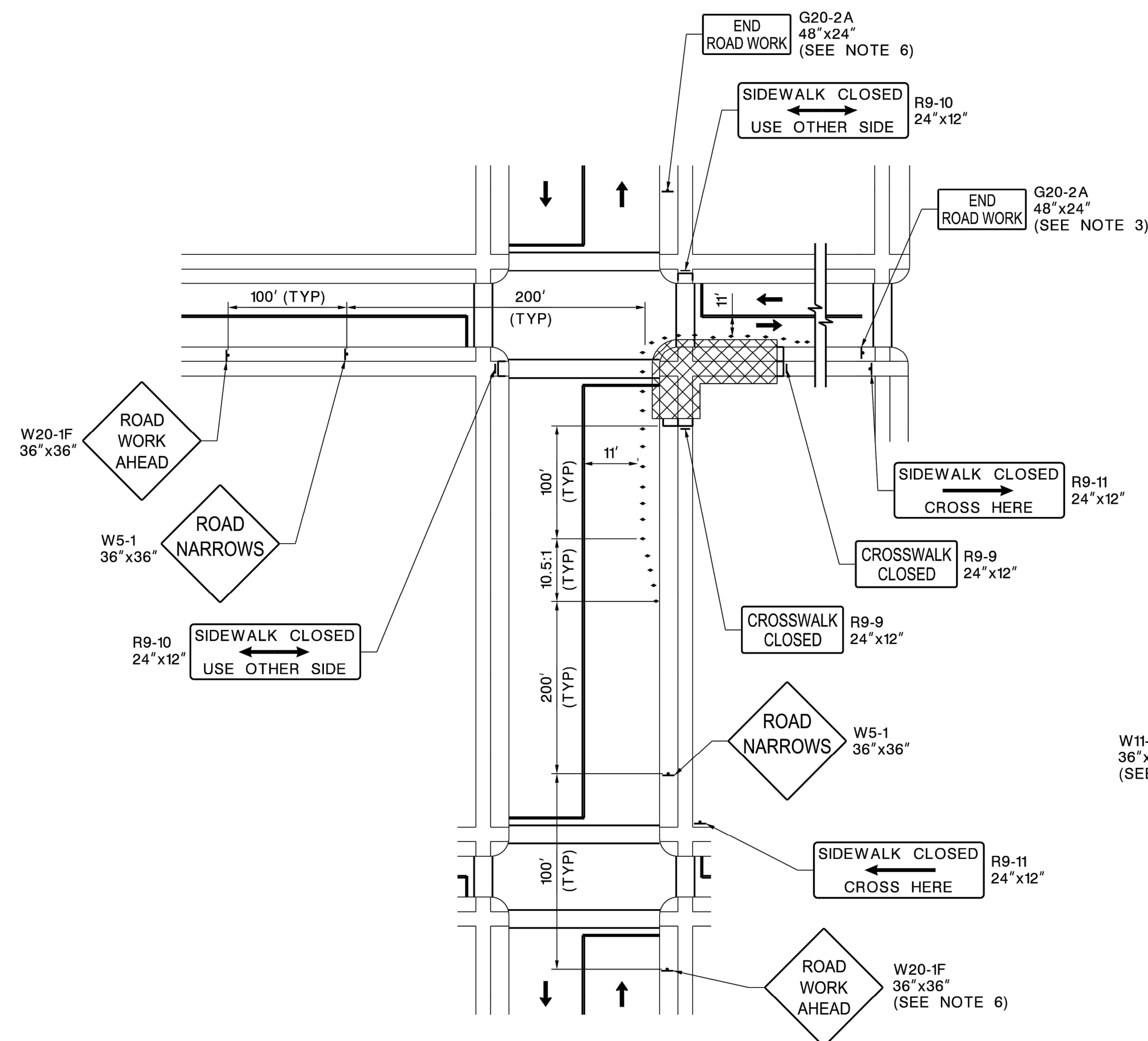
Client: **TOWNSHIP OF CHATHAM MORRIS COUNTY, NEW JERSEY**

Title: **CHATHAM TOWNSHIP PEDESTRIAN SAFETY AND ADA IMPROVEMENTS**

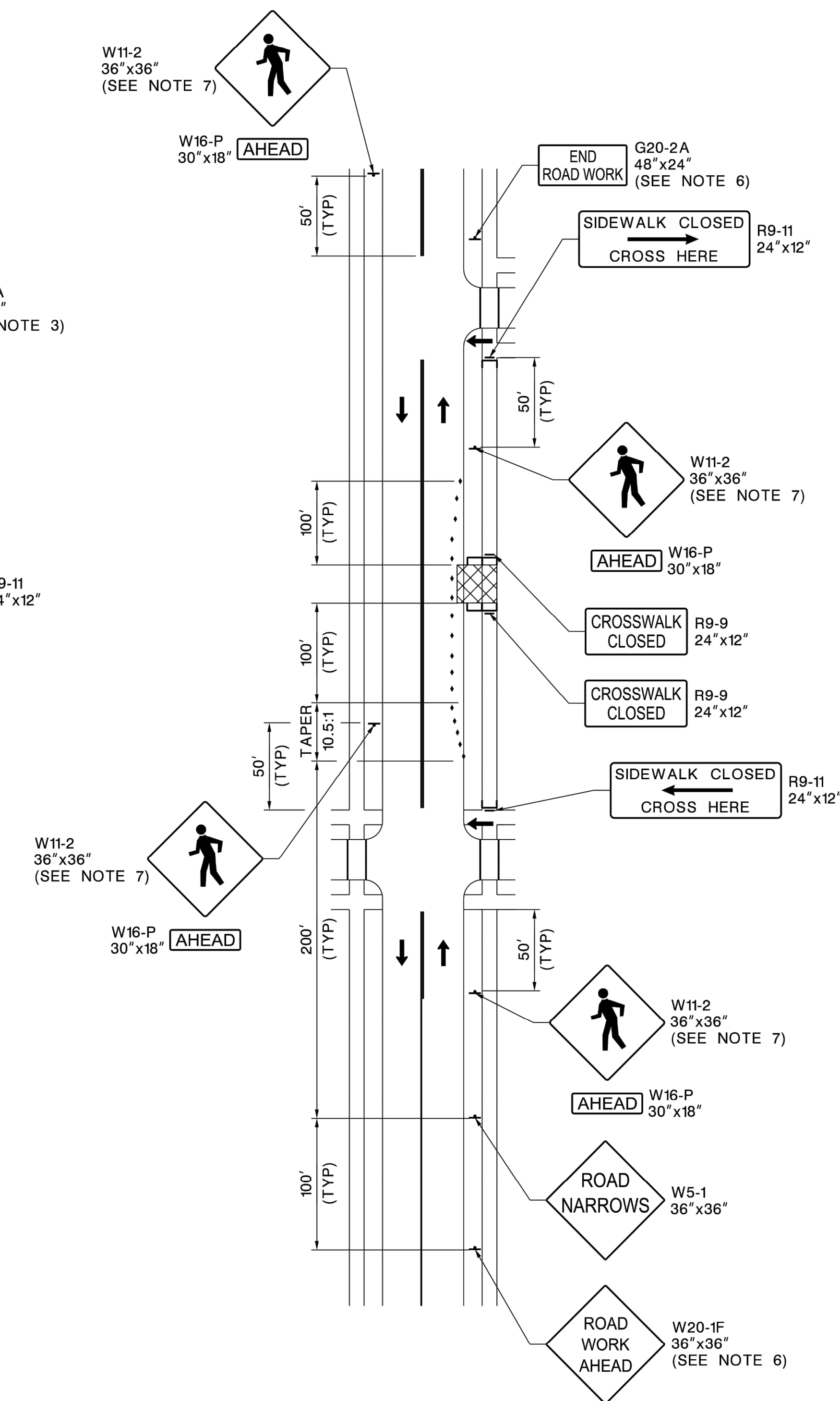
TRAFFIC CONTROL DETAILS

NOTES - TRAFFIC CONTROL FOR SIDEWALK AND CURB RAMP WORK

1. WHEN CROSSWALKS OR OTHER PEDESTRIAN FACILITIES ARE CLOSED OR RELOCATED, TEMPORARY FACILITIES SHALL BE DETECTABLE AND SHALL INCLUDE ACCESSIBILITY FEATURES CONSISTENT WITH THE FEATURES PRESENT IN THE EXISTING PEDESTRIAN FACILITY.
2. TYPICAL DISTANCE BETWEEN CONES IS 12'.
3. THE RECOMMENDED LANE WIDTH IN THE PROXIMITY OF THE WORK ZONE IS 11'. THE TEMPORARY LANE WIDTH MAY BE REDUCED TO 10' WITH THE RE APPROVAL.
4. BEFORE BEGINNING WORK AT ANY INTERSECTION, THE CONTRACTOR SHALL COORDINATE WITH THE BOROUGH TO IMPLEMENT TEMPORARY PARKING RESTRICTION FOR MINIMUM 300' IN ADVANCE OF THE WORK ZONE.
5. MULTIPLE WORK ZONES CAN BE SET UP ALONG THE SAME STREET ONLY IF ADEQUATE PEDESTRIAN DETOUR AND/OR SIDEWALK DIVERSION CAN BE IMPLEMENTED FOR EACH WORK ZONE.
6. INSTALL W20-1 AND G20-2A ONLY AT THE BEGINNING AND RESPECTIVELY AT THE END OF THE CORRIDOR, IF MORE THAN ONE INTERSECTION IS IMPROVED ALONG THE SAME STREET.
7. IF THE APPROACH IS STOP CONTROLLED INSTALLATION OF W11-2/W16-9P IS OPTIONAL.



**TYPICAL CROSSWALK CLOSURE - PEDESTRIAN DETOURS
AND TRAFFIC CONTROL**
NOT TO SCALE



**TYPICAL SIDEWALK DETOURS
AND TRAFFIC CONTROL**
NOT TO SCALE

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Rev	Date	Drawn	Description	Ch'k'd	App'd

JOHN K. RUSCHKE PROFESSIONAL ENGINEER LIC. NO. 24GE0037148 Date 08-03-2023		Project Number 507393101-001	B/O 19	Total 19
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N.T.S.	Draft	Rev0		STD	
Drawing Number		T-005			

Title CHATHAM TOWNSHIP PEDESTRIAN SAFETY AND ADA IMPROVEMENTS	
TRAFFIC CONTROL DETAILS	